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CENTRAL CONCORD REDEVELOPMENT AREA STREET BEAUTIFICATION

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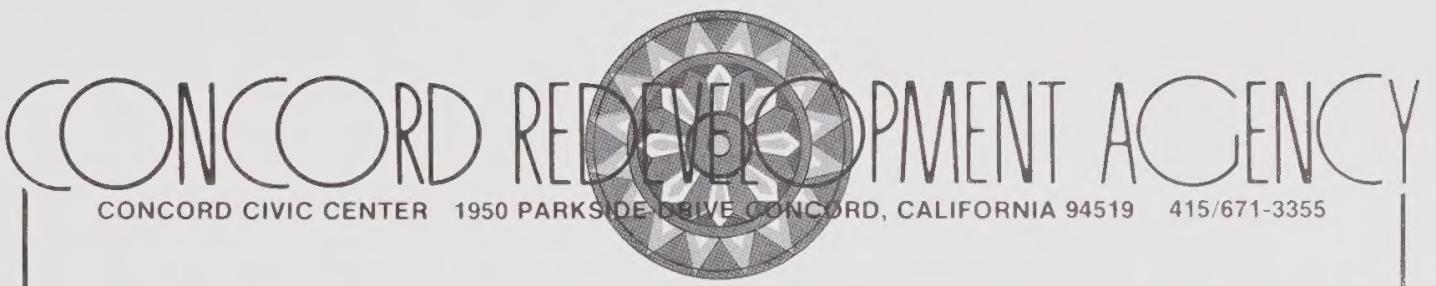


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CENTRAL CONCORD
EXISTING STREETS



December 18, 1984

Dear Reader:

This book contains the Central Concord Redevelopment Area Street Beautification Master Plan and Design Guidelines. The Plan reflects the Redevelopment Agency Board's (City Council's) ongoing commitment to improving the appearance and function of the downtown area.

Implementation of this Plan will enhance the appearance of the major streets within the downtown area, through the addition of landscaping within the public right-of-way and adjacent private property. Toward that end, this Plan serves as a guideline for the City/Agency staff, downtown property owners, and development community in the preparation of improvement plans for major downtown streets, and properties which front on these streets. The Agency Board recognizes that the scope of work involved in improving the appearance of all major streets in the downtown area is large, and cannot be undertaken at one time. Rather it will be necessary to implement this Plan incrementally, over time. This will occur through the construction and reconstruction of streets within the area, and the development or redevelopment of adjacent parcels. A continual commitment to the Plan will be necessary in order to ultimately achieve the desired result.

The Agency Board/City Council would like to thank the Planning Commission, Design Review Board, and and Redevelopment Advisory Committee for the time and effort they put into the development of this Plan. The Board also extends special thanks to POD, Inc. for their professional assistance in developing the Plan.

Sincerely,

Steve Weir
Agency Board Chairman

MY1206:mf/sd

CENTRAL CONCORD REDEVELOPMENT AREA STREET BEAUTIFICATION

A Master Plan Summary and Design Guidelines for
Streetscape Development in the Concord Redevelopment Area

Prepared for
The Concord Redevelopment Agency

Prepared by
POD, Incorporated
San Francisco, California
December 1, 1984

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ACKNOWLEDGMENTS

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PREFACE

Over the past 15 years the City of Concord has experienced tremendous growth both in population and property values. This has occurred partially as a result of the Bay Area Rapid Transit (BART) providing direct linkage to other parts of the San Francisco Bay Area and partially as a result of inevitable development in northern California. To help guide a continued future growth towards an overall high quality image, the Concord Redevelopment Agency retained POD, Inc. to develop a program for its major streetscapes that will reinforce the City identity, provide safety for pedestrians, and be effectively achieved through both public and private funds over a period of years.

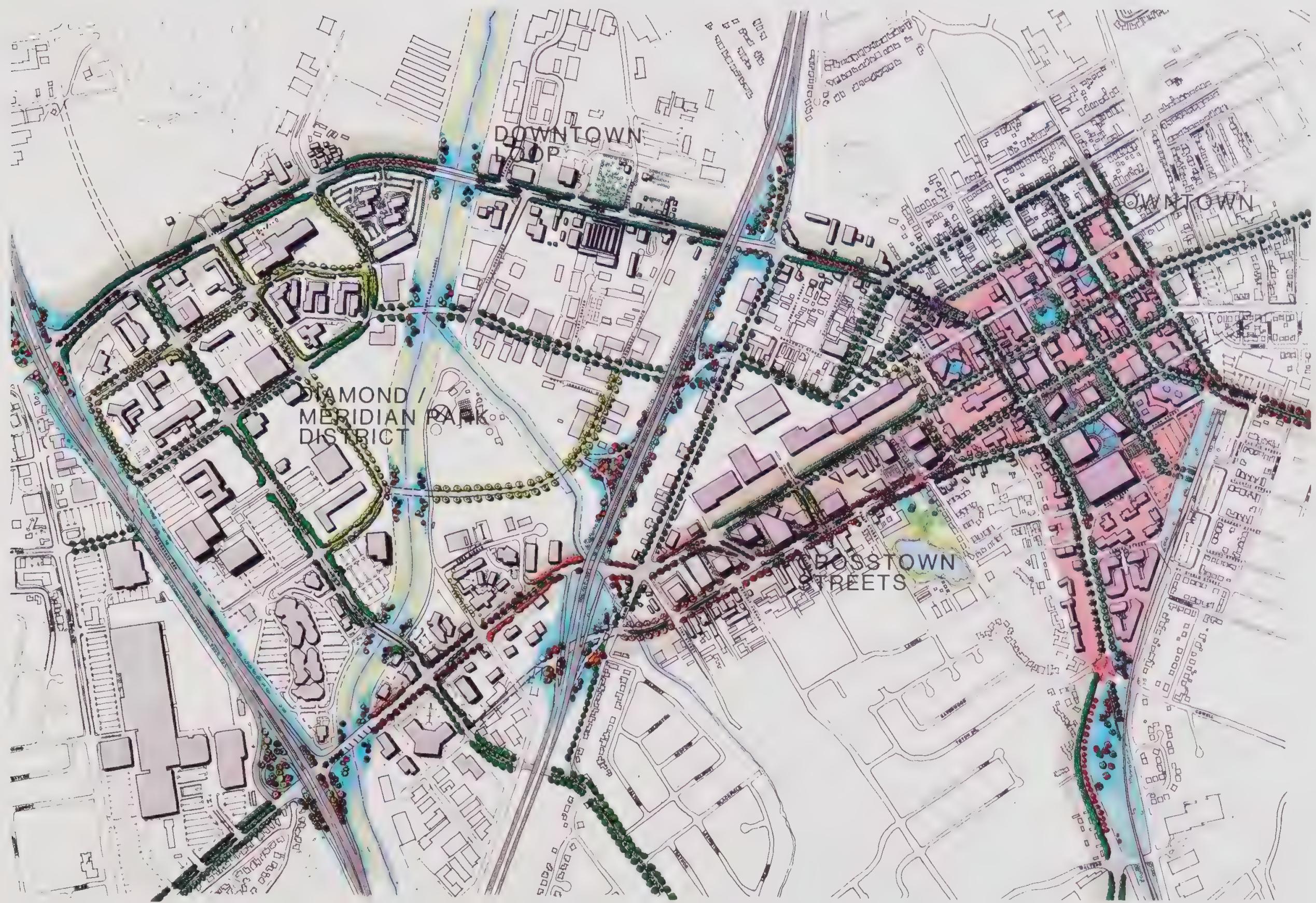
Specifically this report presents a design plan and implementation guidelines for the beautification of major streets and entrances of Central Concord within the Redevelopment Project Area. This area includes Willow Pass Road from Contra Costa Boulevard to Galindo Street, Concord Avenue/Galindo Street from Interstate 680 to Cowell Road; Clayton Road from Highway 24 to Oakland Avenue; Market Street from Concord Avenue to Clayton Road; and all streets in Meridian Park including Diamond Boulevard, John Glenn Drive/Galaxy Way, Burnett Avenue, Meridian Park Boulevard, and Orion Street.

In addition, it is our recommendation that on those streets that continue outside the redevel-

opment area, and therefore the specific scope of the study, the same standards be applied to new development. These include Monument Boulevard (continuation of Galindo Street) from Cowell Road to Interstate 680, and Willow Pass Road from East Street to Parkside Drive.

In order to develop and implement such a plan, it is important to understand how the many elements of the City streetscape including traffic movement, plant materials, walkways, key land uses, significant projects, individual buildings, etc. all function and contribute to an overall image. To this end, the following report is composed of four major sections — I. Analysis, II. Master Plan; III. Design Guidelines; and IV. Implementation — each representing a phase of study and each an integral part of the total recommendation process.

It is a purpose of this report to demonstrate how the streetscape and project interface can contribute positively to the city image and long-term evolution of Concord into a well planned, attractive community. While the conclusions and recommendations of this report suggest a comprehensive approach to the renovation of the streetscape and landscaping, it does not suggest that such improvements are the sole obligation of the City or Redevelopment Agency. The responsibility of implementation and costs should be shared with private development which will likewise benefit from being a part of a beautiful city.



ANALYSIS

ANALYSIS

INTRODUCTION

The visual image of Concord is composed of a complex interaction between private development, building setbacks, site development, city street improvements, and public right-of-way treatment.



The following is a summary of the site inventory and analysis phase of this report. Its purpose is to describe and document all factors that currently influence the visual character of the streetscapes, and in doing so to identify how these contribute to the perceived image of the City.

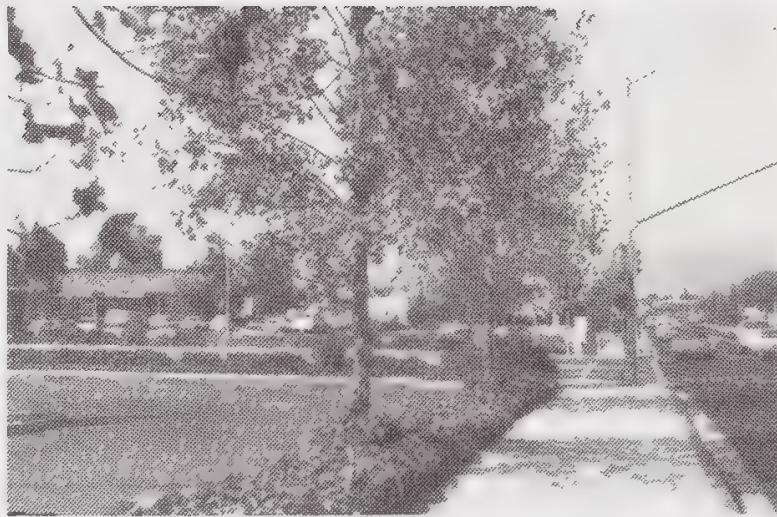
The focus of this study is on the streetscape corridors and much of the emphasis is put on what is visible and perceived by the occupants of the automobile. The ingredients that contribute to one's visual perceptions include: speed of traffic, origin and destination of vehicles, street widths, parkway and sidewalk widths, overhead and underground utilities, plant material size and character, maintenance practices, and the character of land use and development.

The resulting visual image corridor is a complex interaction between private development, building setbacks, site development, city street improvements, and public right-of-way treatment.

Because of this complexity, the beautification of Concord's streets can not be achieved by street tree planting alone. Street beautification for the purpose of upgrading the visual image of the City should include all elements visible from the road on public and private properties as well.

ISSUES, PROBLEMS & OPPORTUNITIES

The “view from the road” is one of the most significant ingredients shaping the ultimate image of Concord.



The parking lot and its visibility is probably the most consistently reoccurring image throughout the City of Concord.



The “view from the road” is one of the most significant ingredients shaping the ultimate image of Concord. Much of the visual perception and awareness of Concord as a city will be from the automobile. Traffic volumes, speed, and complexity of movement are all on the increase in the City.

An example of this is the sequence of driving down Interstate 680, exiting at Willow Pass Road, continuing east past Park and Shop, through the downtown past Todos Santos Plaza and on to City Hall. This experience establishes a visual image and perception of the City of Concord.

Streetscape landscaping and improvements of the public right-of-way are one ingredient of this visual image. Other ingredients include the character of individual site developments, like the Park and Shop or the Willows Shopping Center; architectural scale, color, complexity, and setbacks; and visible site treatments including landscaping, paving, parking and lighting.

Parking of the automobile is a significant use of land. In Concord, as in many similar communities, the amount of land occupied by the automobile comprises more than half the total land surface area for office and commercial development. Added to this are numerous automobile dealerships that line Concord’s streets. As a re-

Each type of land use has a different set of inherent conditions that generate a different physical character.



Streetscape should respond to the unique conditions that occur within the different types of land use.



sult, the parking lot and its visibility from the arterial streets is probably the most consistently recurring image throughout the City of Concord.

In addition to the commonality of the parking lot, Concord is also composed of the following land use types: residential, commercial, office and light industrial. Each of these has a different set of inherent conditions that generate its physical character. For instance, residential has a different scale, density, parking requirement, and massing appearance than commercial or office use. An automobile sales lot has different needs and requirements than those of office and commercial development. Each land use type, therefore, generates a distinctive physical character, each with different problems and potentials.

When combined in larger zones these differences potentially form an important contribution to the scale, variety and uniqueness of Concord. An example of this is the "fabric" or "texture" of the downtown area around Todos Santos Plaza which is entirely different than that of the Meridian Park Area. The former is composed of smaller scale single building varieties, the latter includes large projects that may set a singular image for entire blocks. Through these zones, the streetscape should respond to the unique conditions that occur within the different types of land use.



Setback requirements should include specific treatment recommendations for planting, grading, and lighting.



Street widening or improvements have been made with the single purpose of improving traffic flow and vehicular safety to the detriment of the street's aesthetic value.

Existing planning standards have already been established for the Downtown Business District and for subareas within the District including Todos Santos Plaza, Market Street, and West BART Area Overlays which describe setback distances and height restrictions. These, however, do not specify requirements as to the actual treatment of the setback area. More specific standards should be established to deal with the variety of site and building conditions which occur within the setback zone and adjacent to the public right-of-way. These standards should include specific requirements as to landscaping, lighting, signing, minimum parking lot treatments, service areas, etc.

Additionally, in the past much of the street redevelopment, widening or other improvements have been made with the emphasis on improving traffic flow and vehicular safety. The end result is exactly that — a street that handles traffic flow more efficiently and safely. However, lost in the process is the retention or expansion of an adequate public right-of-way for proper street planting and pedestrian circulation. In many cases too, the adjacent land use has continued to utilize the site in an "as is" condition. Revamping or adjusting of the parking lot, or the adding of landscape or screening has not occurred.

This problem is expanded due to the particular nature of street improvement projects. Street improvement plans address traffic flow and traffic capacity on a larger scale basis than

a few particular sites. The improvements are usually done on an entire street for a length of several blocks or several miles. In contrast to this linear improvement, actual site improvements or redevelopment projects may take place on a smaller incremental, site by site basis. Thus while the street improvements may be completed for the entire length of the street, improvements of the individual sites along the street are more likely to take place over a longer period of time.

Maintenance of street landscaping as well as availability of maintenance funds is another increasingly important factor to the image of a city. The level of maintenance possible will necessarily dictate the type and sophistication of any street beautification program.

In the past, the City of Concord has been following a policy of minimizing landscape maintenance and water requirements wherever possible. This is an understandable direction given the increasing cost of maintenance resources and the shortage of funds available. However, in practice, efforts to reduce right-of-way areas and parkways, the elimination of irrigation and the installation of "low maintenance" or drought tolerant plantings have unfortunately achieved a less than desirable visual result.

Over the years this low, or no maintenance policy has taken its toll on the character of the

Concord streetscape. In a number of situations the initial public investment of street landscaping has been lost as much of the plant material has not survived. Parkways which were minimized to reduce maintenance are either entirely paved or contain plant material which is stunted and/or creating additional problems for curbs and sidewalks due to invasive root growth. Additionally, our research and experience indicate that in an urban environment, parkways and medians do require maintenance to maintain an image consistent with an urban character. The actual cost for this to be done properly seems to be fairly consistent regardless of what is planted.

Today, combined with the desire for this plan, there is a renewed city interest in a high quality, more refined landscape image for Concord's Central Business District. To achieve this there must be a new commitment to street landscaping, irrigation, and beautification along with an equal commitment to proper maintenance. The issues now should be who will maintain the streetscape and who should bear the costs.

Cities throughout California are or have faced this same problem of lack of available resources and funds to care for street landscaping. There are many examples of alternative programs and methods of achieving desirable results. These usually involve having property owners pay a proportionate share of the installation and/or

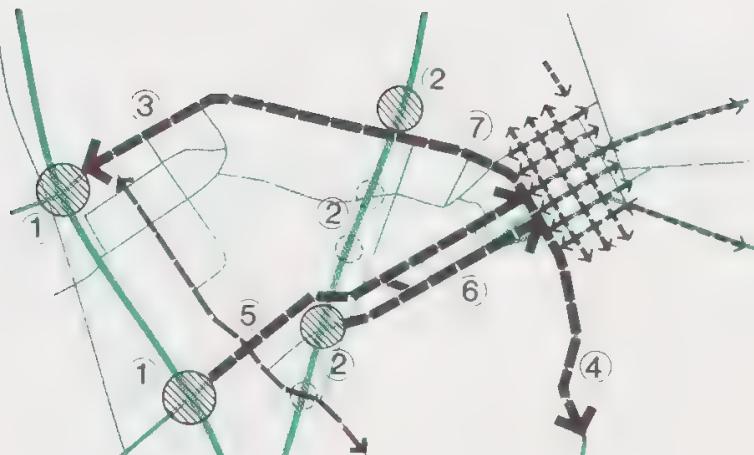
maintenance costs for the entire area.

In private development, "associations" are now common in business parks, residential and commercial projects. The association, made up of the property owners, assumes responsibility for ongoing care of the project after initial installation has been completed by the developer. The City of Concord has already used the concept for the maintenance of landscaping in new housing subdivisions.

Cities have developed the establishment of "assessment districts" where property owners are assessed a proportionate amount of the installation costs within a defined area, and "maintenance districts" where property owners are assessed for their proportionate share of the maintenance costs. In addition, Concord has used "maintenance agreements" for private commercial development to maintain landscaping within the immediately adjacent, right-of-way.

In the following pages, the individual aspects of the analysis including circulation, land use/visual fabric and street improvements are reviewed in further detail. Existing setback requirements are summarized in the Appendix section of this plan. Existing landscape conditions are summarized in the Existing Conditions Sections, also located in the Appendix. The interdependence and interaction of these factors result in the analysis conclusions (page 13) and the summary diagram that follows.

CIRCULATION



Traffic volumes and patterns are an important aspect of this study. The traffic flow on the arterial streets create corridors of movement and therefore, corridors of visual image. The heaviest travelled streets are correspondingly the streets with the most potential for establishing a significant visual image. Major elements of the circulation patterns for Central Concord are as follows:

Interstate 680 [1] is an intraregional freeway link and is a prime physical factor in influencing development in Concord; however, very little of Concord can be seen from it. The areas of most significance from a visual standpoint are at the points of access, i.e. the interchanges. These points of egress serve as major entrances to the City of Concord.

Highway 24 [2] is presently undergoing a study to revamp its Concord offramps and access points. When completed it will function more as a local expressway with multiple points of connection to the surface streets. The new Clayton Road offramp as well as the Concord Avenue exit will also be important entrances into the city.

The Concord Avenue [3]/Galindo Street/Monument Boulevard [4] loop forms a primary linkage from Interstate 680 and Highway 24 passing through the downtown core and will carry significant volumes of traffic.

Willow Pass Road [5] together with Clayton Road [6] will function as a parallel linkage from Interstate 680 and Highway 24 to the Downtown and beyond, and is also planned to carry large volumes of traffic.

In the downtown zone [7] bounded by Galindo Street, East Street, Pacheco and Oak Streets the corridor nature of travel suddenly changes. Traffic is dispersed to smaller, often one-way, streets. The traffic becomes more diverse, with slower speeds and shorter destinations. Parking and pedestrian traffic also increase in this zone.

A network of secondary streets including Diamond Boulevard, the proposed Galaxy Way and Market Street accommodates local travel within Central Concord.

STREET IMPROVEMENTS



The City of Concord has projected certain street improvement projects on a priority schedule as summarized in the adjacent diagram. The indicated street improvements include, among other things, widening the streets and/or medians. These can be regarded as both an opportunity and a constraint.

The opportunities exist in two situations. Where no improvements are anticipated landscape and streetscape efforts and investments can proceed without the concern of future street construction damaging the landscape improvements. In the situation where street widening is anticipated in the immediate future, the opportunity exists for a coordinated effort to achieve both the road improvements and streetscape beautification simultaneously. Where the road improvements are tied to a site or project redevelopment, the opportunity is even greater since a total coordinated effort between Public Works planning and private development must occur.

The constraints exist where the street will be widened in the future from 1 to 5 years or even beyond 5 years. Any landscape improvements accomplished in these areas will most likely be removed as parts of the new street construction. Any streetscape or landscape improvement in these areas should be planned and coordinated with the traffic engineering requirements. An attempt should be made to design improvements so that they will be only minimally disturbed by the future road modifications.



The City of Concord is composed of several distinctive land use characters or textures. These can be summarized in land use zones and are visible as districts or neighborhoods.

The largest and least well defined is the light industrial/office/manufacturing mixed use area starting at Diamond Boulevard and extending up to Highway 24 and across Concord Avenue [1]. This area is characterized by predominantly low rise, large scale site and building developments.

The residential zone [2] is primarily on the outskirts of the study area and is located southeast of Clayton Road and surrounds the north, east and southeast portions of downtown. This area is characterized by a mixture of multi-family and single-family residential areas. The variety of construction materials, scale of buildings, architectural diversity and density all contribute to a small-scale texture.

In contrast to this are three large-scale projects that are visually identifiable and act as reference points to most of the citizens in Concord.

The most isolated one is the Sun Valley regional shopping mall, located west of Interstate 680 [3] and composed of large scale monolithic architectural shops. It is distinctive due to its size and mass rather than to any other factor.

The Meridian Park area [4] includes the Willows specialty retail center, Chevron Card Center and the Concord Hilton Hotel. This area is isolated by Interstate 680 and the flood control channel, but is identifiable as a new, well landscaped complex.

The Park and Shop center [5] is dominant along Willow Pass Road largely due to its length. The present image is a low complex of miscellaneous shops and stores floating in a large parking lot. As landscape improvements are accomplished in the parking lot, this image should soften and improve. Given its central location, it will remain a significant and visual reference point to the City of Concord.

The most distinctive zone is the immediate downtown area in the blocks surrounding and including Todos Santos Plaza [6]. The area is characterized by small square block configurations and the recent renewal efforts of the Todos Santos Plaza and streetscape. Future projects such as the Bank of America complex and other redevelopment efforts will increase the visibility and importance of this zone as a downtown center. In addition, the BART Station is an important element from both a physical and socioeconomic standpoint. As the downtown area expands, BART should visually and functionally become more a part of this zone.

The freeway corridors of both Interstate 680 and Highway 24 [7] are important visual ingredients and are most predominant at the overcrossings and interchanges. These play an important reference role when arriving or leaving the City.

Also, between Highway 24 and Market Street there is a zone of predominantly vacant land in the abandoned railroad right-of-way. While this land is not particularly visible at present, if development occurs along this stretch, it could become a major ingredient shaping the image of Central Concord. The architectural and landscape character should be carefully controlled as it relates to the view from Highway 24 between the Clayton Road and Concord Avenue interchanges.

The two waterways that run north south through Concord, and most particularly the Walnut Creek Channel, are important open space elements within the City. Views and planting should be emphasized where they cross the roads. There is also a potential for more active recreational ingredients in the fabric of Concord, including a recreation/pedestrian trail system.

ANALYSIS CONCLUSIONS

Two primary elements which work together to establish the visual image of Central Concord are circulation and land use character. The circulation analysis shows that the primary surface streets are the “Loop” (Concord Avenue, Galindo Street, and Monument Boulevard); and the “Crosstown” connection from the freeways to the central downtown area (Willow Pass Road/Clayton Road). As these streets pass through the different land use areas, the general character and site conditions that are displayed along them form an important public image of Concord.

The streetscape beautification plan should therefore, be developed to enhance and express this interaction. Landscape development along the key streets could greatly upgrade and reinforce the city image, and also help establish a sense of organization within Central Concord.

Following is a summary of the different zones of visual importance along the major circulation routes. These are also illustrated on the following page.

1. The points of entry from the freeway systems are the initial visual introductions to the City of Concord. The character and quality of landscape of the interchange areas as well as the immediately surrounding land uses and architectural/landscape treatments are important in establishing the visual introduction to Concord.
2. Concord Avenue, Galindo Street and Monument Boulevard function as a crosstown loop connection from Interstate 680 to the downtown center. Although the streets serve many points of destination along their entire length, the most dominant traffic pattern and visual linkage is between the freeway intersections and the downtown core.
3. Both Willow Pass and Clayton Road with their ultimate traffic improvements will function as the most important crosstown linkage from the freeway to downtown and then east to the Concord Pavilion. They will serve as the major access streets for commercial and office facilities presently envisioned for this area.

Also portions of Willow Pass Road and Clayton Road immediately adjacent to the downtown area are functionally becoming extensions of this commercial and office district. Maintaining the physical and visual quality of the downtown core in these areas would be desirable to successfully establish this connection.

4. A transition occurs where linkage corridors (Concord Boulevard, Monument Boulevard, Willow Pass Road, Clayton Road) meet the edges of the downtown core. There is potential for the intersections of the two to serve to introduce the central core and to define decision points for drivers with differing destinations.
5. The downtown core or district has a unique quality, scale, and clear identity. It is composed of small blocks, the historic Todos Santos Plaza, and several restoration and redevelopment projects. The core area, more than any other, stands out as the positive and quality image presently identified with Central Concord.



MASTER PLAN

MASTER PLAN

INTRODUCTION



The Master Plan, supported by the Design Guidelines, sets forth a plan to define and integrate visual aspects of Concord's streets. It is intended to serve as the beginning and a basis for long term beautification of the Concord streetscape.

The primary purpose of the Master Plan is to provide basic concept recommendations and an overall program which defines the general structure to which all individual private and public sector efforts and decisions can relate. The plan must function and adjust over a long time period and is not meant to be a fixed and unchanging document.

The actual achievement of the goals and objectives of the Master Plan will be the result of a combined effort from both city originated projects such as road and right-of-way improvements, and of private development efforts as new projects or renovations occur. The Master Plan and Guideline sections of this report are intended to direct these efforts in a common, mutually supportive direction.

Elements of the Master Plan include a discussion of the overall concept, specific streetscape concepts, and potentials for feature areas or gateways. The Master Plan also addresses the importance of individual sites and components such as furnishings, walk dimensions, setback areas, etc. to the overall plan.

CONCEPT

The Master Plan is based on the previous analysis and the conclusion that the visual image and character of Concord can be significantly improved by aesthetic development along its major traffic arterials.

The concept's most basic recommendation is that the City should reinforce and strengthen its image with a landscape development program and plan establishing a sequence of positive images linking freeway access points to the downtown core.

Specific recommendations of the Master Plan are as follows:

1. Develop Concord Avenue, Galindo Street, and Monument Boulevard as the "Crosstown Loop". The primary visual character of the streets should achieve a clear and positive linkage from the 680 freeway access to the downtown core.
2. Develop Willow Pass Road and Clayton Road as the major "Crosstown Street" linking the downtown to Interstate 680 and Highway 24. This includes establishing a



visual framework which integrates all existing and proposed development along this corridor.

3. Develop and enhance the access points of the Interstate 680 interchanges as regional "City Entries" to Concord.
4. Develop and enhance the local access points along Highway 24 in a similar fashion to the Interstate 680 interchanges.
5. Develop "Downtown Gateways" to mark arrival points to the downtown zone in six major locations.
6. Develop Diamond Boulevard, Market Street and Galaxy Way as secondary streets for local circulation within Central Concord.
7. Establish Meridian Park as a visual district including landscaping of medians and parkways.
8. Expand the already successful streetscape improvements in the Todos Santos area to include the larger downtown bounded by Pacheco Street, Adobe/Mira Vista, Amador Ave./Galindo, the BART tracks and Port Chicago Highway. Also develop Willow Pass Road along the Park and Shop frontage up to Galindo Street as an extension of the commercial-office character of the Todos Santos area. (Note: There is currently a study for Concord in process that examines this downtown area in more detail. See Reference List in Appendix.)



ILLUSTRATIVE MASTER PLAN

FEATURE AREAS

The Master Plan recommendations on the preceding pages fall into two main design categories:

1. Feature Areas
2. Streetscapes

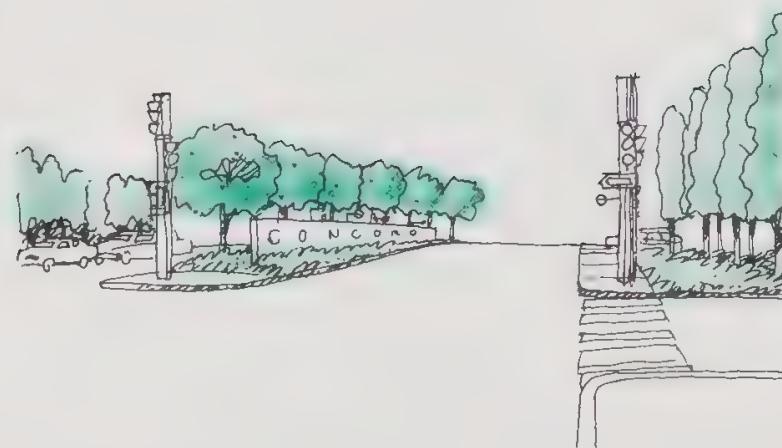
The first of these, "Feature Areas", is further subdivided into two categories to include City Entries and Downtown Gateways. Both are special locations or identity points where distinctive designs can dress up and/or give identity points to Central Concord by highlighting unique aspects of the City. An example of this potential is shown, for one location, to the left in a before and after study sketch sequence.

The discussion that follows describes both the Entries and Gateways in concept only. The recommendations are general and attempt to describe the "spirit" or intent of the zone rather than the specific design.

As each project moves towards actual implementation, a specific landscape program and design should be developed to incorporate thematic, aesthetic, utility, safety and cost concerns.

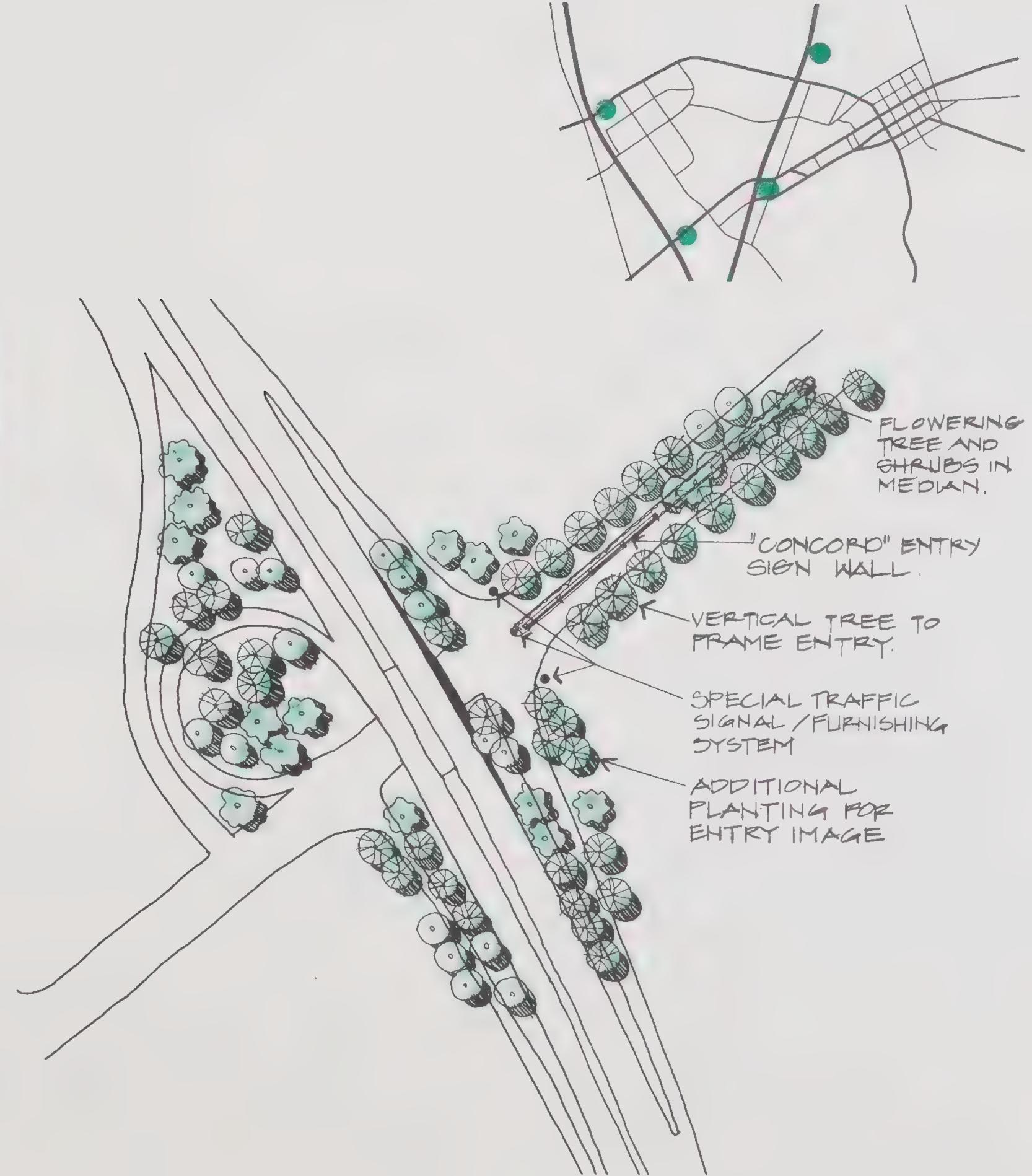


FEATURE AREAS: CITY ENTRIES



The City Entries are located where one exits regional freeway systems and enters onto the major Concord streets leading toward the downtown and the Concord Pavilion. One's first impressions of the city are formed at these nodes. The major entries are identified at the junctions of Interstate 680 with Concord Avenue and with Willow Pass Road; and also along Highway 24 at the future Clayton Road and Concord Avenue exits.

Common characteristics of these entries should include the City of Concord sign, a strong vertical landscape tree statement framing the gate, canopy trees with a variety of flowering shrubs in the medians to display seasonal color and a distinctive traffic signal furnishing system. Traffic signals, street signs, lights, etc. should be integrated and designed so that they enhance and contribute to the overall city image. A prototypical example is shown in the illustrations on this page.



FEATURE AREAS: DOWNTOWN GATEWAYS

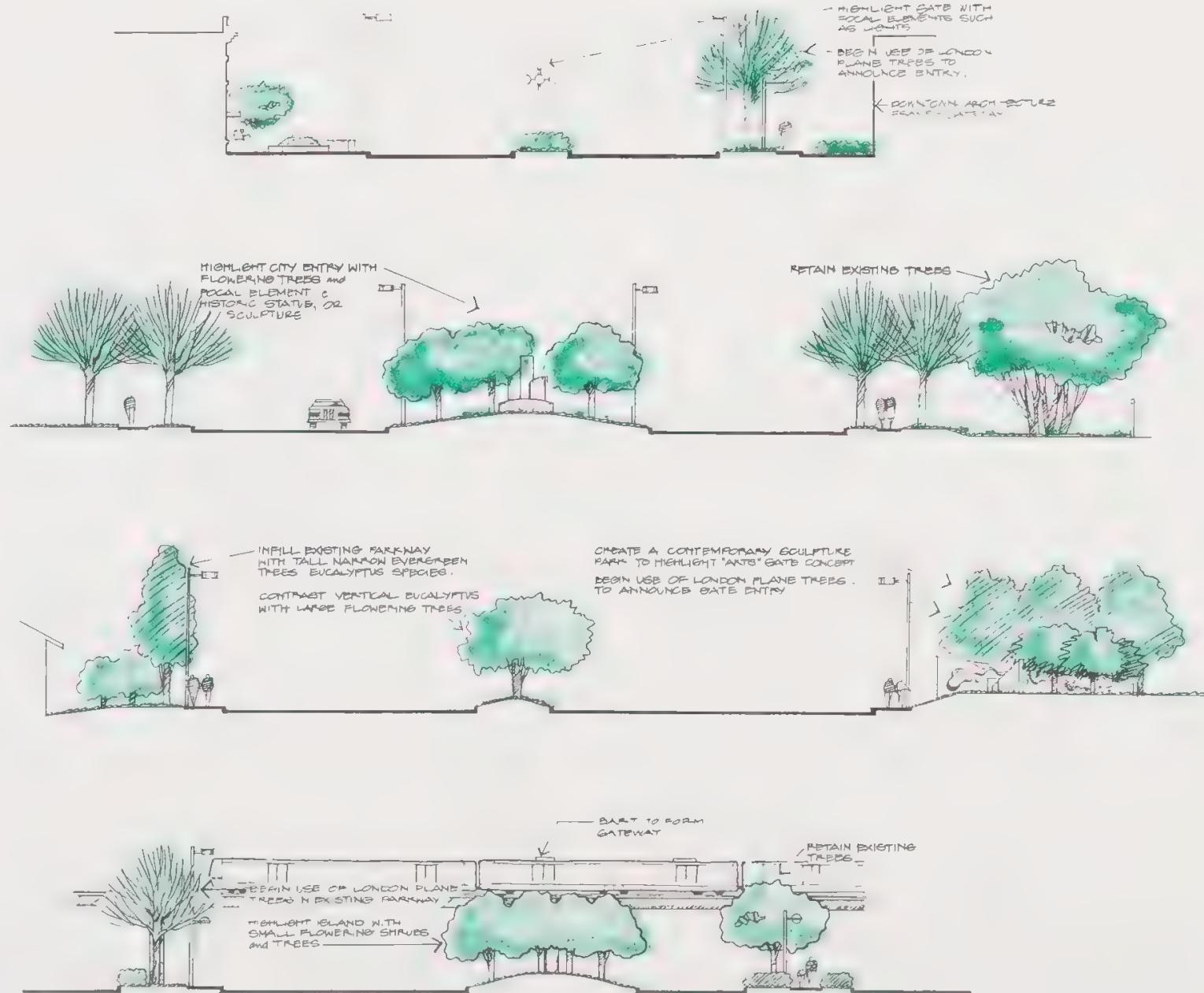
The “Downtown Gateways” occur at the junctions of the “Linkage Streets” and the enlarged “Downtown” zone. These are symbolic entries to the heart of the city, each intended to carry a different theme representing aspects of Concord. Examples of these might be Fanfare Gateway, Historic Gateway, Arts Gateway, BART Gateway, Community Gateway, and Civic Gateway.

The Gateways are to be focal points for the City characterized by distinctive architecture, planting, lighting, enriched paving, and special feature elements as may be appropriate to the theme, i.e., sculpture, fountains, etc.

The illustrations to the right identify conceptual ideas for individual gateway themes. Specific themes and design solutions should be selected by the City and prepared by designers at the time of implementation for each location. For specific feature elements the City may even look to a larger community involvement through an Arts ordinance, grants, competitions, and/or contributions by private sources.



CONCEPTUAL GATEWAY EXAMPLES:





DOMINANT LANDSCAPE ELEMENTS

- ① DOWNTOWN LOOP Tall evergreen skyline tree, deciduous medians, broad parkway.
- ② CROSSTOWN STREET Formal broad canopy tree, tall evergreen in medians.
- ③ DOWNTOWN Continuation of canopy plane trees, urban street furnishings and plazas, wide sidewalks, shrub plantings and/or tree wells.
- ④ SECONDARY STREETS Informal canopy tree plantings, walks at curb with berms and/or shrubs to screen parking.
- ⑤ MERIDIAN PARK Smaller parkway width and tree sizes, informal plantings,

The second aspect of the Master Plan addresses the linear aspects of the “Streetscape”. The design objective here is to define enough ingredients for each street to provide continuity and consistency in an otherwise unstructured environment.

Important elements include the median width and planting, street tree, sidewalk width and location, street light poles and furnishings, street signing, parkway planting, and visible elements of adjacent land uses.

Specifically, one component of the streetscape concept is to develop a strong and consistent image along each of the major street systems while still encouraging some degree of variety. This is accomplished by identifying a dominant street tree and different median tree for each system.

Ultimately these trees would comprise approximately 70% of all trees along each particular street. The remaining 30% would be existing trees, accent trees planted at project entries or plaza areas, and other trees mixed into the streetscape edge for variety.

A second goal is to establish a hierarchy and difference between major collectors, secondary streets, local streets and the downtown area. This shall be achieved by first identifying contrasting tree forms for streets of similar importance, i.e., canopy for the “Crosstown Street”

and vertical upright trees for the “Downtown Loop”; and second, by establishing different minimum parkway widths and configurations for the different street types. These would include an urban edge (10' sidewalk at curb) for the Todos Santos area, the major boulevards for the Crosstown Street and Downtown Loop (generous parkway and street tree area, sidewalk, then planting beyond), and a smaller scale streetscape planting for secondary streets (smaller parkway or sidewalk at the curb, then tree and landscape zone beyond).

In addition, parking lots and all visual clutter should be screened from the road, either by architectural or landscape means. This includes trash and service areas on private property, and can be accomplished by berms, hedges, low walls, etc. In the special case of automobile sales lots, we are recommending at least a partial screen of either a lower hedge or a “window” into the site for approximately 40% of its street frontage, with the remainder screened as described above. Signage, as well, should be tasteful and controlled by city ordinances.

This Master Plan supports the general ideas presented in the Master Plan for Street Trees for the Major Thoroughfares for the City of Concord prepared in 1979 by Mai Arbegast for the City with the following exceptions:

1. As discussed in the Analysis section, there will be a level of maintenance required in-

cluding automatic irrigation to achieve the quality of image Concord is striving to attain. Therefore the plant list, including some specifically drought tolerant species, may have to be adjusted accordingly.

2. To firmly plant an identity, this plan proposes a stronger single dominant street tree statement for the major street systems (i.e., Crosstown Street, Downtown Loop) as opposed to the large variety proposed in the Arbegast report. There would still be, however, variety included for each street beyond the dominant street tree.

Implementation of this Master Plan can be achieved through the adoption of these concepts and enforcement of Design Guidelines that define the specific criteria for each of the various streetscapes and their components.

DESIGN GUIDELINES

DESIGN GUIDELINES

INTRODUCTION

The Master Plan proposes that streetscape districts or zones be established for the primary visual corridors throughout Central Concord and that City entrances be established at the junctions of these districts.

In this section, each streetscape is further defined in its role in providing a visual identity for Concord. Furthermore, the Design Guidelines provide specific criteria as to street furnishings, landscape palette, and right-of-way requirements.

The ultimate achievement of the Master plan is a long-term effort and is based on the success of implementing and enforcing the Design Guidelines on a day-to-day basis.

The Design Guidelines outline specific performance goals and/or criteria that will enable the City to direct and orchestrate incremental improvements over the years ahead. Reinforcing the Master Plan, the Guidelines are intended as a tool to assist both the City of Concord and the private landowner in long-term efforts of street beautification.

The majority of streetscape improvements will occur on private property as a part of the overall site design for new projects. These projects should adhere to the criteria set forth in this section for that portion of the site that relates to the streetscape image as defined by this report.

Additional streetscape beautification may occur jointly between public improvements and private development as a result of proposed street widening or as special demonstration projects undertaken by the City of Concord.

The following Design Guidelines demonstrate various conditions of streetscape improvements which would occur on public property, on private property, or in landscape/walk easements. Following the Guidelines is a discussion of the specific streetscape elements. In addition, the “Implementation” section of this report defines an approval process for the City to insure the effectiveness of these Design Guidelines.

DESIGN GUIDELINES: ORGANIZATION

The Design Guidelines are organized to specifically describe each of the streetscape districts discussed in the Master Plan section of this report including:

- I. Downtown
 - a. Todos Santos Area
 - b. Willow Pass Road
- II. Linkage Streets
 - a. Downtown Loop
 - b. Crosstown Street
- III. Secondary Streets
 - a. Diamond Boulevard
 - b. Galaxy Way
 - c. Market Street
- IV. Meridian Park
 - a. Meridian Park Boulevard, John Glenn Drive/Galaxy Way, Burnett Avenue, Orion Street

Each of the above are illustrated on the adjacent map and described in detail on the following pages. The Guidelines are specific and describe the minimum criteria that must be met as well as specific furnishings and plant materials for each street.

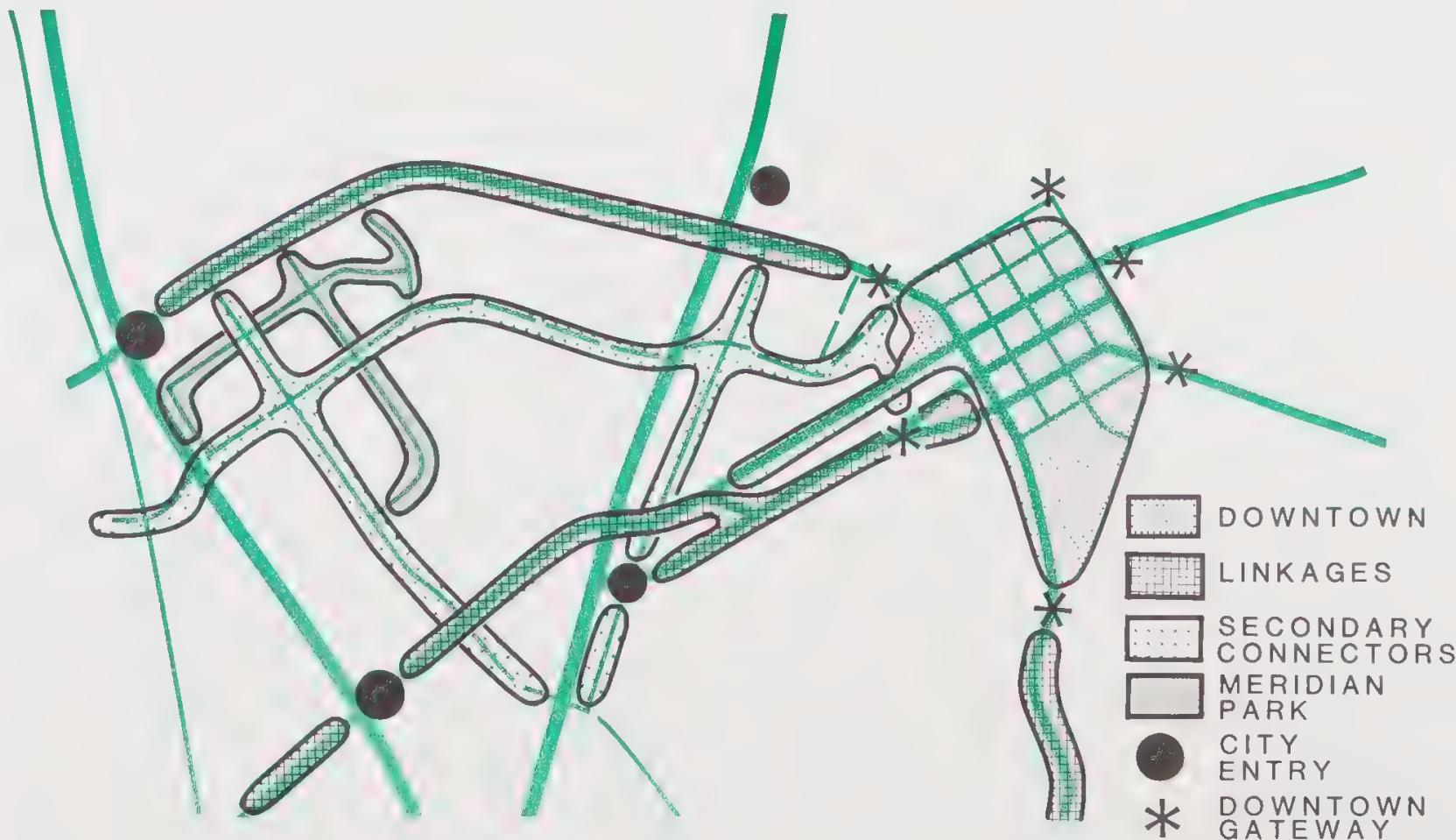
In all cases there is an illustration of the preferred street section shown and several alternatives. These alternatives are to be used only when existing conditions render the preferred section infeasible. When other variations occur that do not fit the above (i.e., conflicts between

tree locations and underground utilities), the City and Design Review Board should enforce the concept by allowing a variation to the preferred section that retains the preferred character as much as possible. Also these preferred sections and criteria refer to "minimum" landscape treatments. Where more land is available the same intent can be carried out in more dimensions and with additional plant materials and varieties.

It is intended that over time incremental develop-

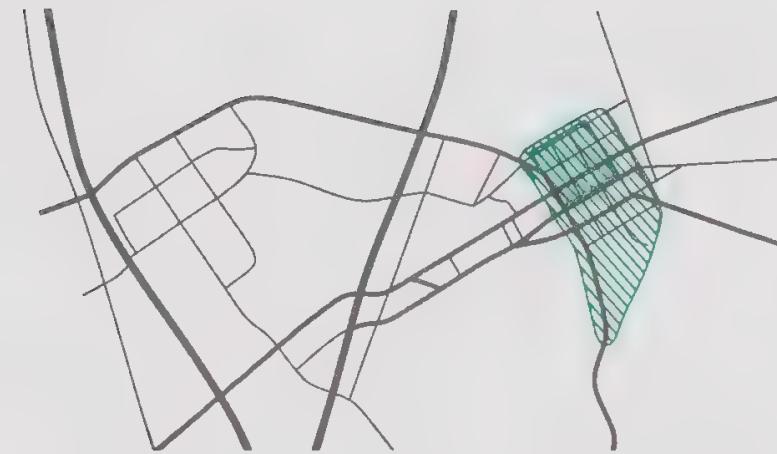
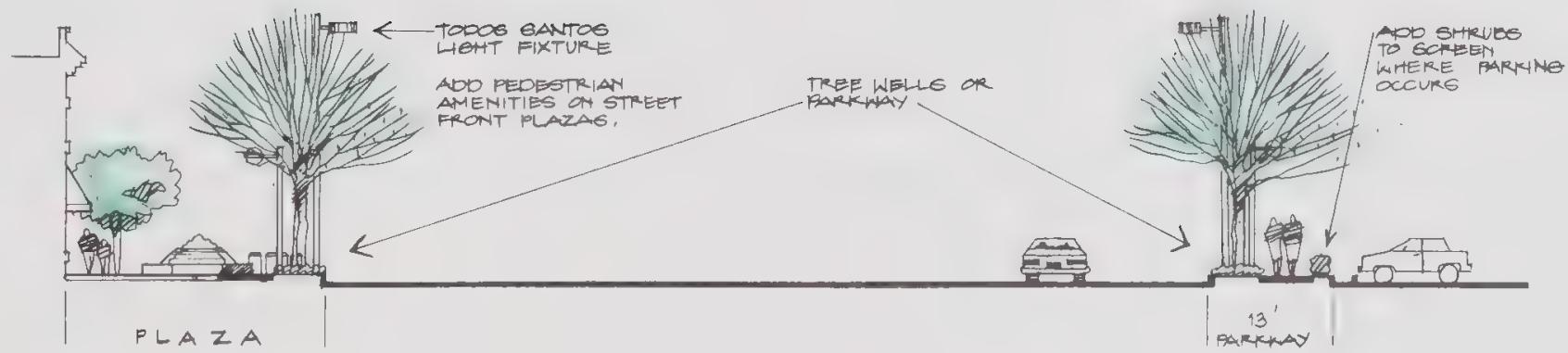
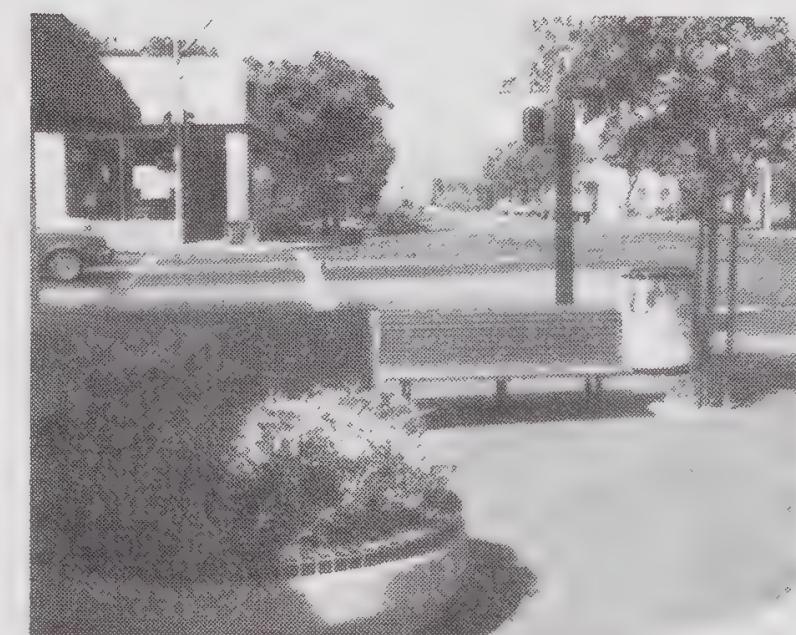
ment following the specific streetscape criteria, combined with the "Feature Areas" described in the Master Plan, will give the streets of Concord distinctive identities and an overall quality image.

The additional three sections of the Design Guidelines (Planting Criteria, Site Furnishings, and Maintenance) describe materials, techniques and/or details common to all streetscape improvements. These sections should be referenced in conjunction with the specific district description for each project site.



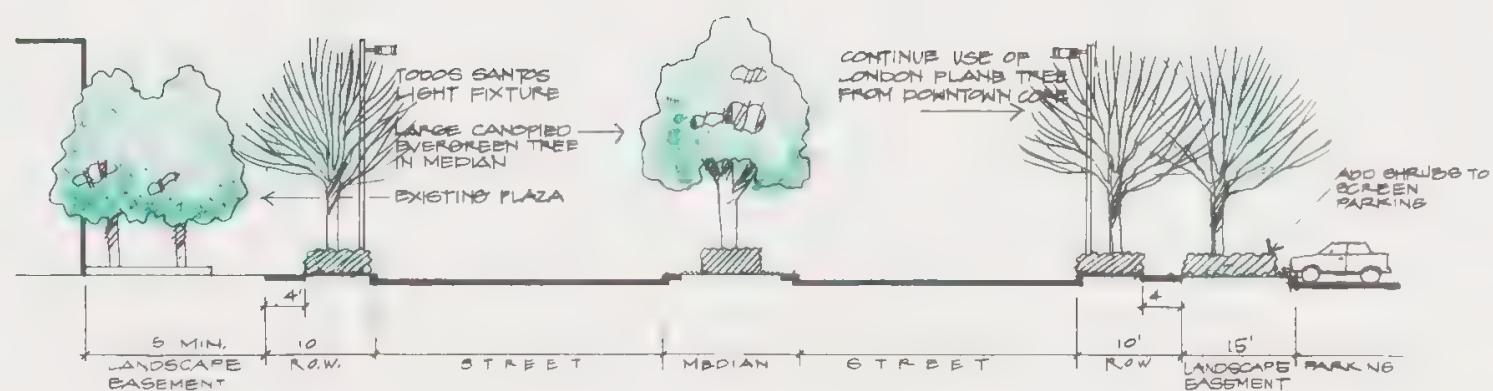
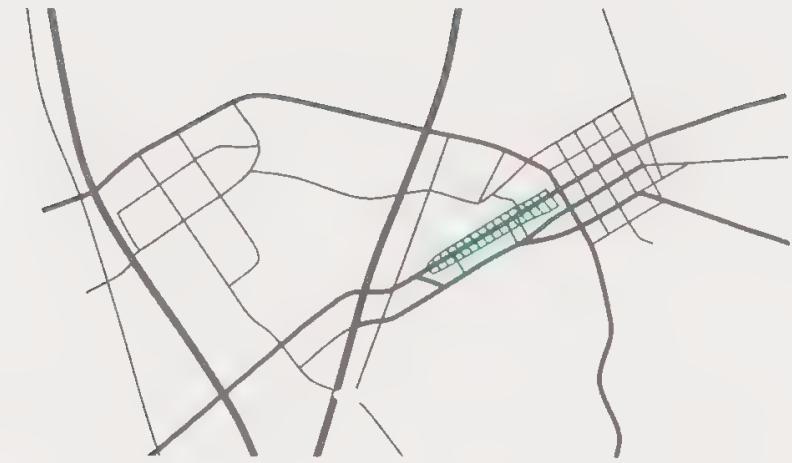
I. DOWNTOWN: a. TODOS SANTOS AREA

In the early 1970's the Todos Santos areas was improved and upgraded by the City of Concord. While specific guidelines for the downtown zone are not the scope of this report, it is recommended that the character already established be continued in the larger downtown area. The Todos Santos area is characterized by a finer pedestrian scale than the outer "linkage" boulevards. Walks are wider, building setbacks are closer to the street and there is more variety in the planting, street furnishings, pedestrian paving, etc. In addition, the historic character of the Todos Santos Park and certain buildings have been preserved for contemporary use while still providing the opportunity for new development in other portions of the downtown. The selection of light fixtures, benches, bollards, etc. helps to unify and enhance the downtown zone by giving the Concord downtown its own distinctive character.



I. DOWNTOWN: b. WILLOW PASS ROAD

This portion of Willow Pass Road extends from the downtown west to the Detroit Avenue intersection where the Crosstown traffic is routed to Clayton Road. Identified as a major commercial center of Central Concord, it is the intent to carry the quality of the downtown commercial area onto Willow Pass and to encompass the area surrounding the Park and Shop and its neighbors. While the streetscape setbacks and landscape areas are broader than the remainder of the downtown zone, this section of Willow Pass Road will retain the feature elements including plant material ("Bloodgood" London Plane street trees), light fixtures, and street furnishings as in the core downtown area, combined with the parkway configurations of linkage streets.



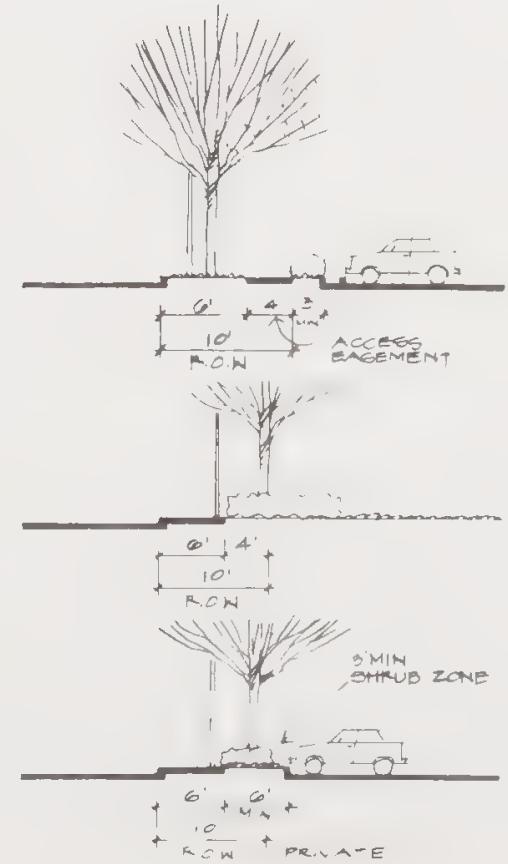
A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Partial redevelopment of site. 15' landscape setback is not possible.

Existing walk is at curb with landscape or building beyond 10' R.O.W.

Existing walk is at curb with parking beyond parkway.



SPECIFIC CRITERIA:

WILLOW PASS ROAD

(Between Detroit Avenue and Galindo Street)

R.O.W.:

Street and Median plus Adjacent 10'

EASEMENTS:

4' walk easement on private property (see opposite page for variations)

PARKWAY:

SIDEWALK

4' minimum separated from curb; 6' when located at curb (see prior page)

TREES

Dominant:

Platinus acerifolia

"Bloodgood"

(Bloodgood Plane Tree)

Deciduous canopy trees

- 24" box minimum in group of 25' on center; 70% of total trees
- 5' minimum from street curbs
- 3' minimum from walks and other curbs
- Infill among healthy existing trees
- Use accent trees at project entries

SHRUBS

30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage

MEDIAN:

TREES

Pyrus calleryana "Aristocrat"

(Ornamental Pear)

Flowering canopy tree

- 24" box
- 5' from face of curbs minimum
- 20' on center

SHRUBS

Ligustrum "Swanee River"

Evergreen hedge under 30" in height; maintain as hedge

GROUNDCOVER

Duchesnea indica

(Indian Mock Strawberry)

Trachelospermum jasminoides

(Star Jasmine)

Strawberry dominant;
Plant jasmine in islands without shrubs

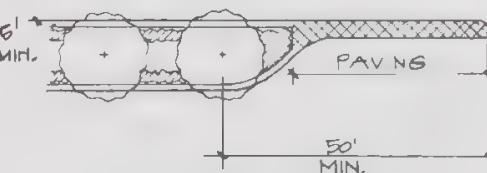
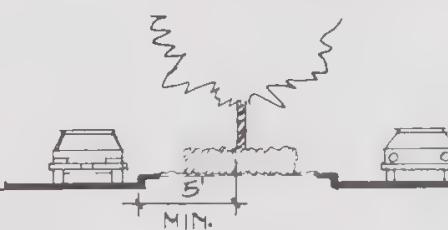
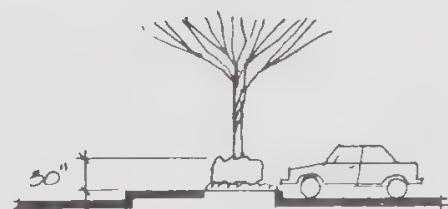
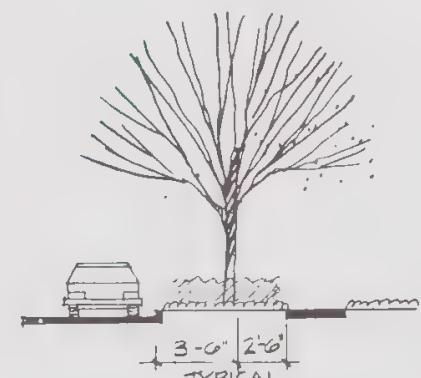
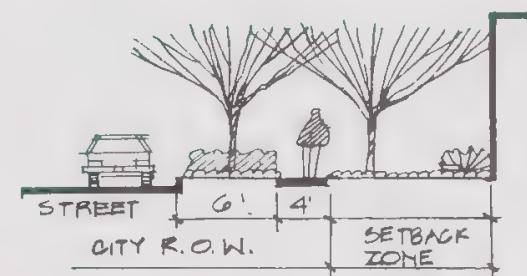
LIGHTING:

Todos Santos Fixture

- Round cut off fixture
- 30' steel pole blue (painted)

PAVING:

Concrete walks, concrete at median nose ends.

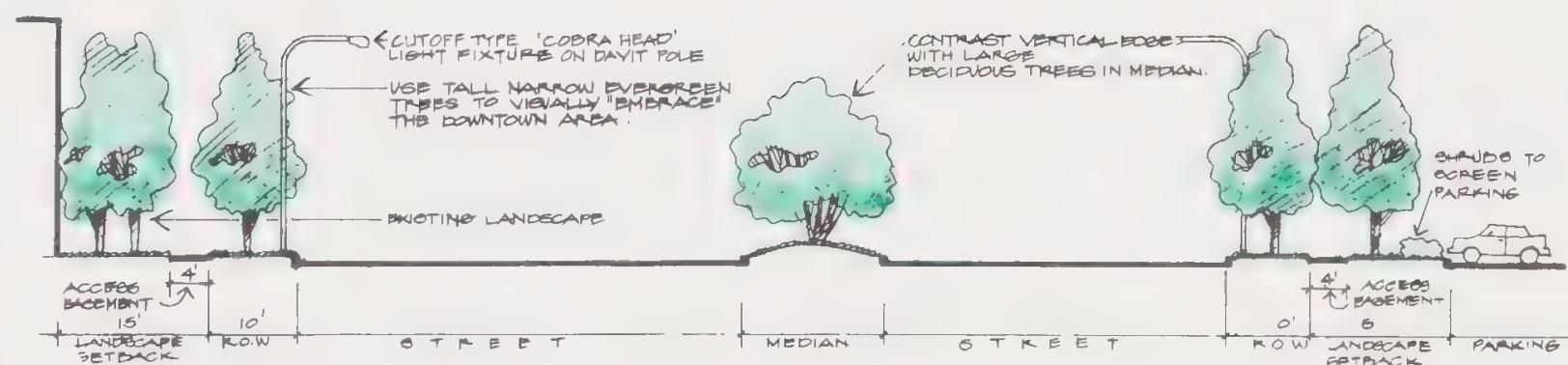
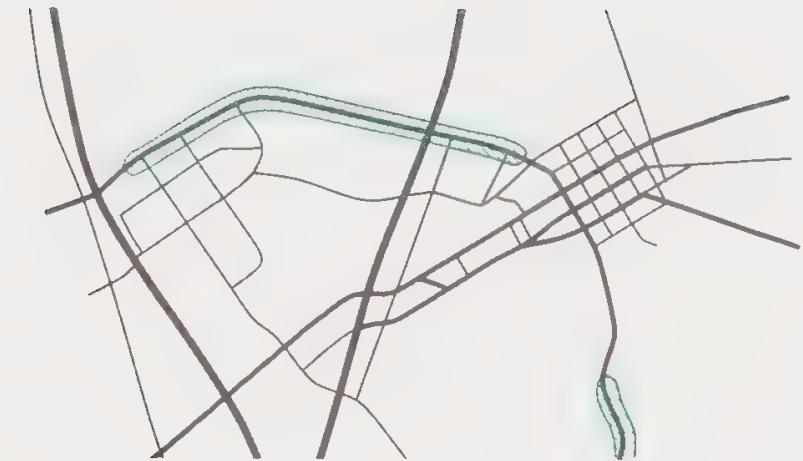


*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

II LINKAGE STREETS: a. DOWNTOWN LOOP

The Downtown Loop consists of Concord Avenue from Interstate 680 to the Downtown Core and Galindo Street/Monument Boulevard south to Interstate 680. It is a major collector surrounding Central Concord that travels from freeway to freeway and passes through the heart of the Downtown and Todos Santos areas. Defined as a "linkage" street connecting the City entries to the Downtown, it is seen primarily as a higher speed through street than the Downtown street network.

The streetscape character is that of a landscaped boulevard with landscaped medians and parkways. Sidewalks separated from the curb with a planting area and additional landscape to the inside of the sidewalk complete and enhance the view from the road. The dominant street features include Eucalyptus as the primary street tree in the parkways, contrasted by many existing Oak canopy trees in the medians and varieties of special tree plantings at individual driveway entrances. The ground plane is characterized primarily by lawn with flowering shrubs used as accents. Medium sized evergreen shrubs will screen parking and service areas.



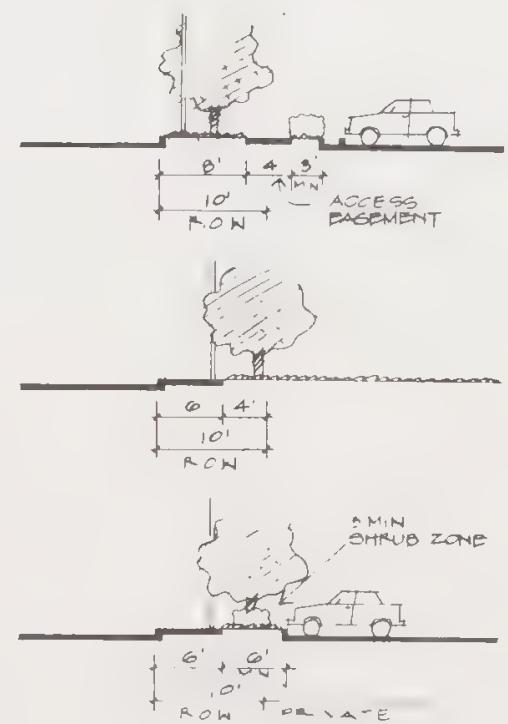
A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Partial redevelopment of site. 15' landscape setback is not possible.

Existing walk is at curb with landscape or building beyond 10' R.O.W.

Existing walk is at curb with parking beyond parkway.



SPECIFIC CRITERIA: DOWNTOWN LOOP

R.O.W.:

Street and Median plus Adjacent 10'

EASEMENTS:

4' walk easement on private property (see prior page for variations)

PARKWAY:

SIDEWALK

4' minimum separated from curb; 6' when located at curb (see prior page)

TREES

Dominant:

Eucalyptus melliodora

Eucalyptus melliodora "Rosea"

Tall vertical evergreen

- Minimum - 15 gal. at 15' - 20' spacing; 70% of total trees
- 5' minimum from street curb
- 3' minimum from walks and other curbs
- Among healthy existing trees infill with Eucalyptus
- Use accent trees at project entries

SHRUBS

30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage

MEDIAN:

TREES

Quercus agrifolia (Coast Live Oak)

Quercus coccinea (Scarlet Oak)

Quercus suber (Cork Oak)

Crown grade to 15% maximum side slopes

Large canopy trees - deciduous and evergreen

- 15 gal. and 24" box, random spacing
- 5' from face of curb minimum
- Planted in clusters of 3 trees minimum of same species, 15' - 25' between trees within each cluster
- Low level irrigation to protect existing Oaks

SHRUBS

Planting under 30" high in median noses 4' and wider; infill existing shrubs west of Walnut Creek Channel

GROUNDCOVER

Turf

Baccharis pilularis "Twin Peaks"
(Dwarf Coyote Brush)

Turf dominant; plant groundcover (baccharis) west of Walnut Creek Channel

LIGHTING:

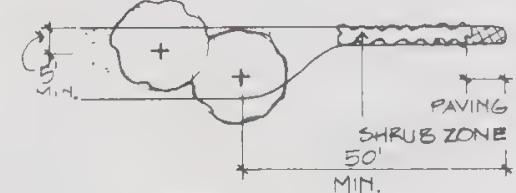
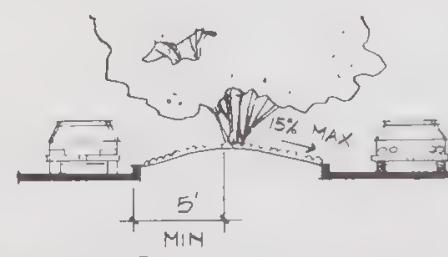
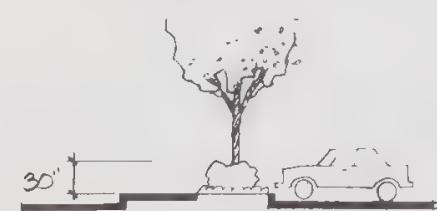
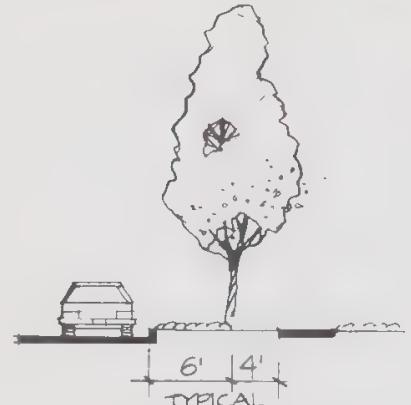
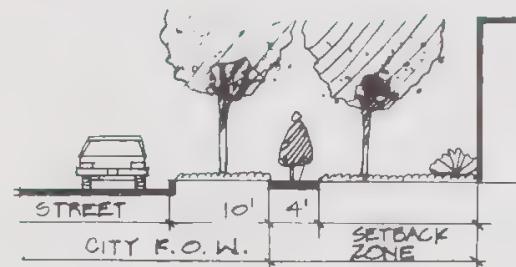
City standard (new)

- Cutoff type "Cobra Head" fixture
- 30' galvanized steel pole with davit arm

PAVING:

Concrete walks, concrete at median nose ends.

*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

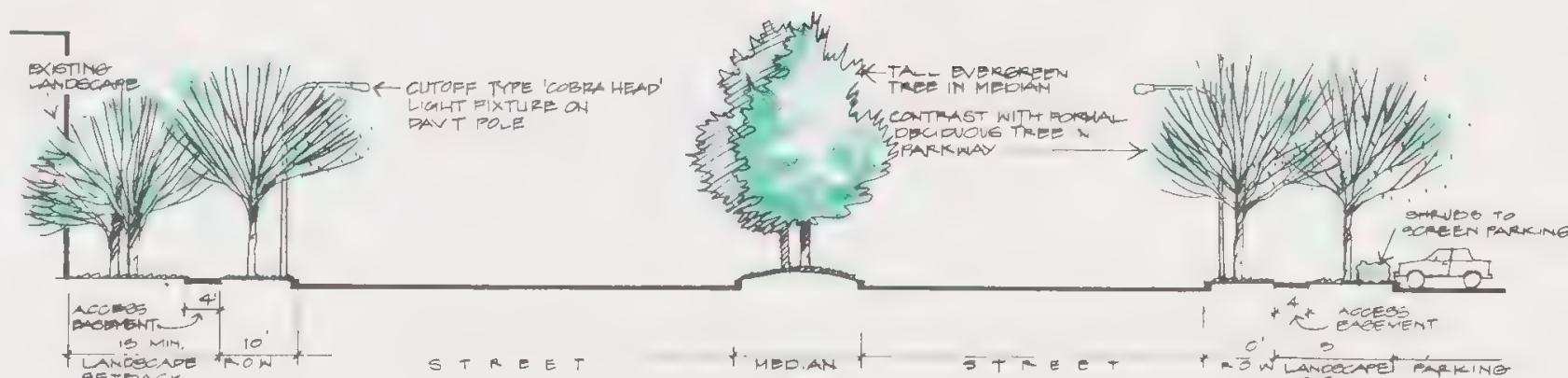
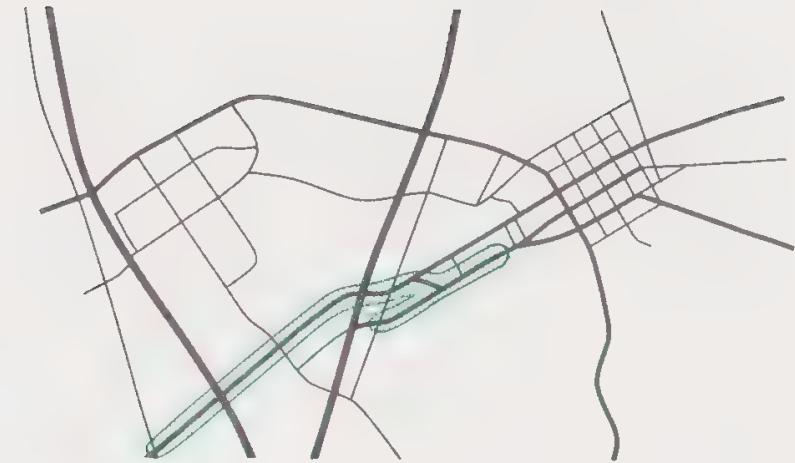


II. LINKAGE STREETS:

b. CROSSTOWN STREET

The “Crosstown Streets” consist of portions of Willow Pass Road and Clayton Road. Similar to the “Downtown Loop”, together they are seen as a major “linkage” or boulevard pair that travel from Contra Costa Boulevard past Interstate 680 and Highway 24 through the center of commercial Concord to the downtown area, and then continue out from the downtown toward the Concord Pavilion. This pair is also characterized by broad landscaped medians, parkways, a separated sidewalk from curb, and additional landscape areas to create a strong image from the road. Traffic signals and signing, particularly at Willow Pass Road and Highway 24, should clearly reinforce the major crosstown function this system serves.

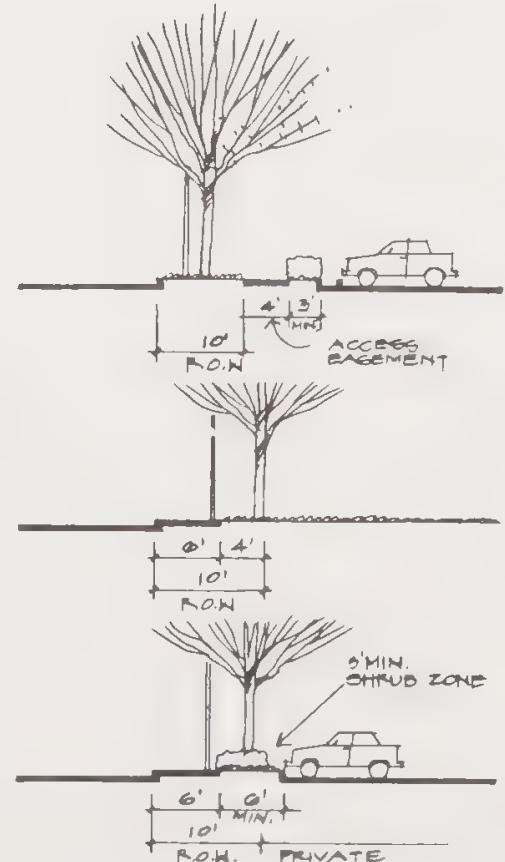
While lawn and flowering shrubs also dominate on the Crosstown Street, this corridor will have an image distinct from the “Downtown Loop”, as the dominant tree species here is changed to Raywood Ash which will ultimately form a broad canopy over the street.



A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Partial redevelopment of site. 15' landscape setback is not possible.



Existing walk is at curb with landscape or building beyond 10' R.O.W.

Existing walk is at curb with parking beyond parkway.

SPECIFIC CRITERIA: CROSSTOWN STREET

R.O.W.:

Street and Median plus Adjacent 10'

EASEMENTS:

4' walk easement on private property (see prior page for variations)

PARKWAY:

SIDEWALK

10' of R.O.W. plus landscape setback zone

4' minimum separated from curb; 6' when located at curb (see prior page)

TREES

Dominant:

Fraxinus oxyacarpa "Raywood"
(Raywood Ash)

Deciduous canopy tree

- Minimum - 24" box at 25' on center; 70% of total trees
- 5' minimum from street curb
- 3' minimum from walks and other curbs
- Among healthy existing trees infill with *Fraxinus*
- Use accent trees at project entries

SHRUBS

30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage

MEDIAN:

Crown grade to 15% maximum side slopes

TREES

Pinus canariensis

(Canary Island Pine)

Liquidambar styraciflua

Tall evergreen tree with deciduous for variety

- 15 gal. and 24" box
- Random spacing, 15' - 25' between trees
- 5' minimum from face of curb

SHRUBS

Planting under 30" in height in noses 4' and wider; infill existing shrubs where spotty

GROUNDCOVER

Vinca minor

(Dwarf Periwinkle)

Groundcover dominant

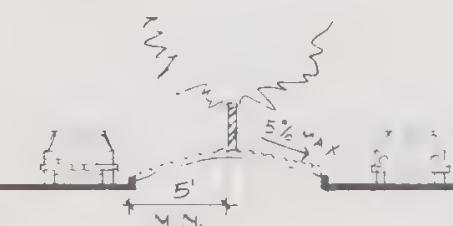
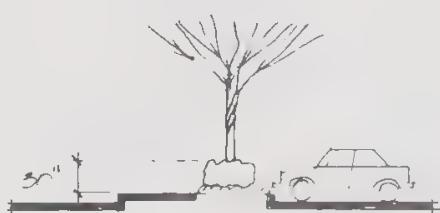
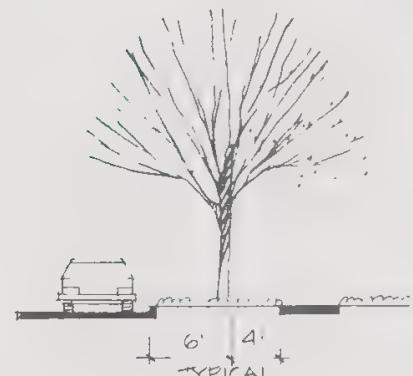
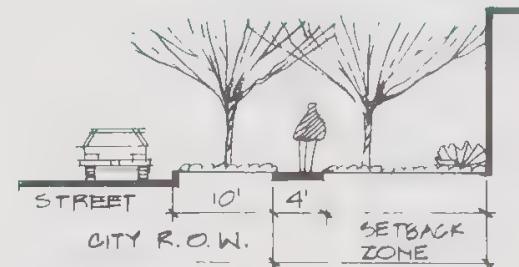
LIGHTING:

City standard (new)

- Cutoff type "Cobra Head" fixture
- 30' galvanized steel pole with davit arm

PAVING:

Concrete walks; cobbles in mortar at median nose ends.

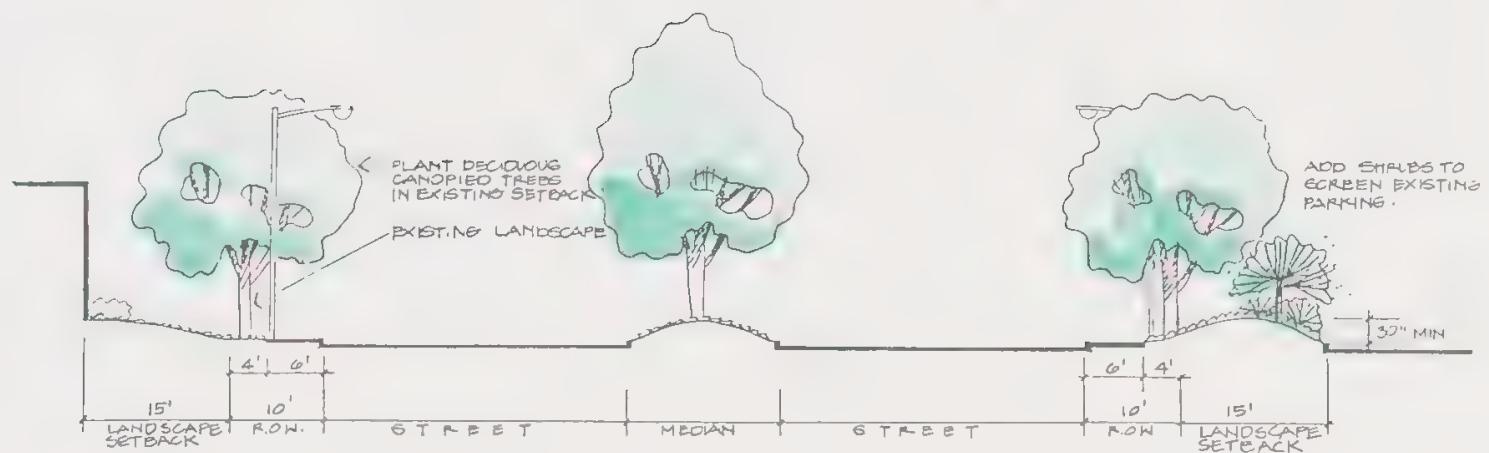


*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

III. SECONDARY STREETS: a. DIAMOND BOULEVARD

The majority of Diamond Boulevard has already been improved with generous landscape treatment setting the direction for the balance of the street. In general, the street is lined with large projects and serves as a secondary connector between the "linkage" streets and eventually will extend all the way south past Highway 24. The sidewalks are located at the curb and are backed by generous grass berms that screen adjacent parking. The dominant street tree is the Evergreen Ash.

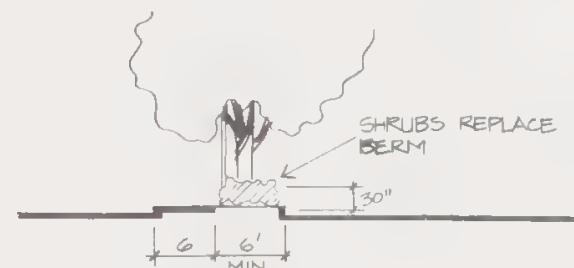
The guidelines for Diamond continue this character and also establish minimum walk dimensions, lighting, and screening conditions for specific situations.



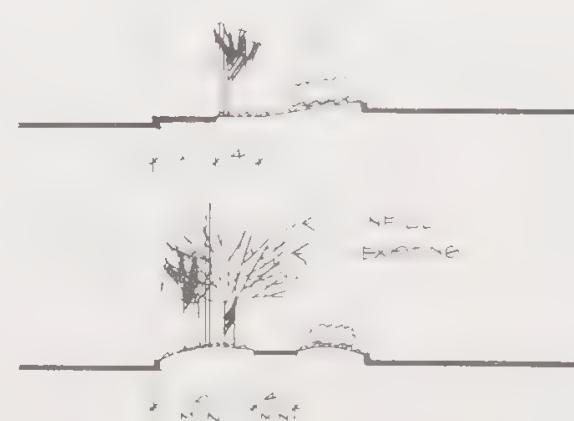
A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Existing condition does not allow space for berm.
Shrub screens parking.



Add shrubs to existing landscape to screen parking.



Existing walk meanders. Infill trees where spacing is greater than 50'. Add shrubs to screen parking.

SPECIFIC CRITERIA: DIAMOND BOULEVARD

R.O.W.:

Street and Median plus Adjacent 10'

PARKWAY:

10' of R.O.W. plus landscape setback zone

SIDEWALK

6' located at curb; 4' minimum when separated from curb (see prior page)

TREES

Dominant:

Fraxinus uhdei
(Evergreen Ash)

Evergreen canopy tree

- 15 gal. and 24" box at 30' on center maximum spacing; 70% of total trees
- 3' minimum from walks and interior curbs
- 5' minimum from street curbs
- Infill among healthy existing trees
- Use accent trees at project entries

BERMS or SHRUBS

30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage

MEDIAN:

TREES

Alnus rhombifolia (White Alder)
Accent Trees

Crown grade to 15% maximum side slopes

Vertical deciduous tree with accents

- 15 gal. and 24" box, random spacing
- 5' from face of curbs minimum

SHRUBS

Planting under 30" in height in noses 4' and wider; infill existing shrubs where spotty

GROUNDCOVER

Turf dominant except at shrub zones

LIGHTING:

City standard

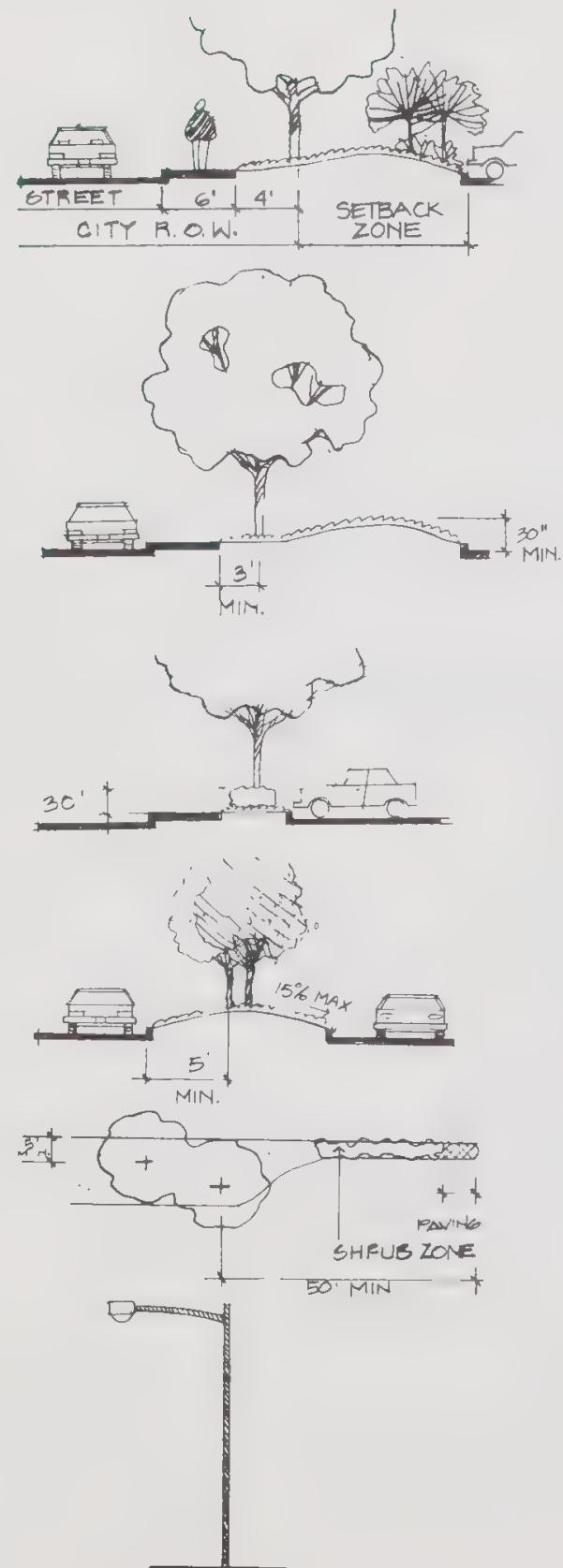
- "Cobra Head" fixture
- 30' galvanized steel pole

PAVING:

Concrete walks, concrete at median nose ends

UTILITIES:

Relocate underground



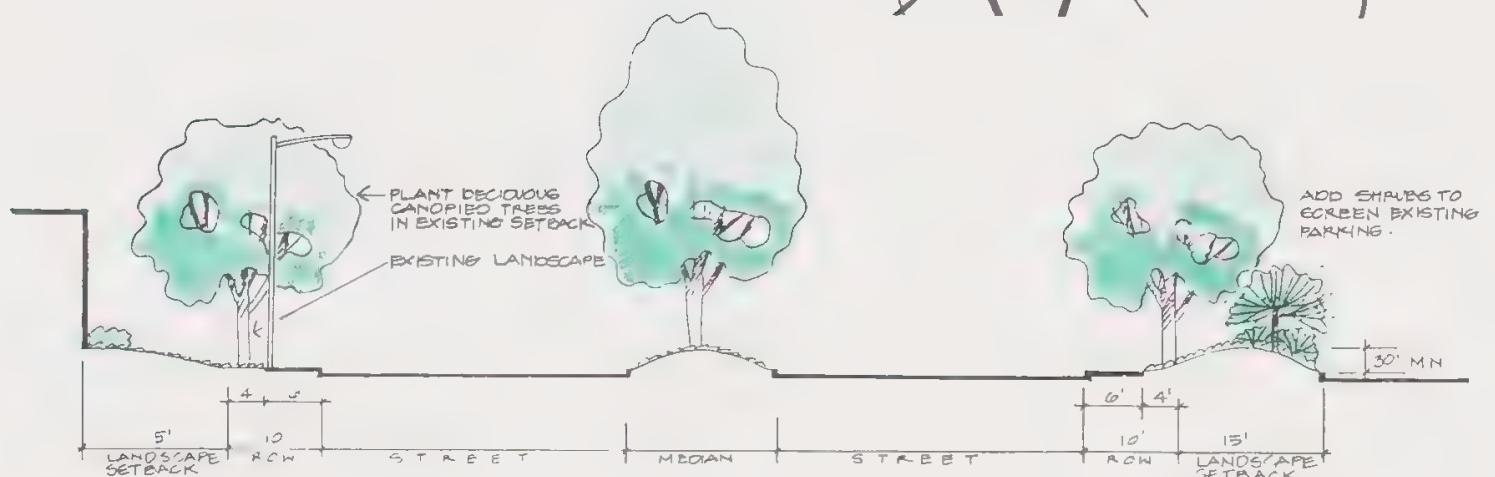
*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

III. SECONDARY STREETS:

b. GALAXY WAY

Galaxy Way is a street that is projected, in future street improvement plans, to extend from Contra Costa Boulevard to the downtown through a primarily industrial/commercial zone.

Treatment recommended for Galaxy Way is similar to previous two secondary streets. The dominant street tree is changed to give Galaxy Way its distinct identity.



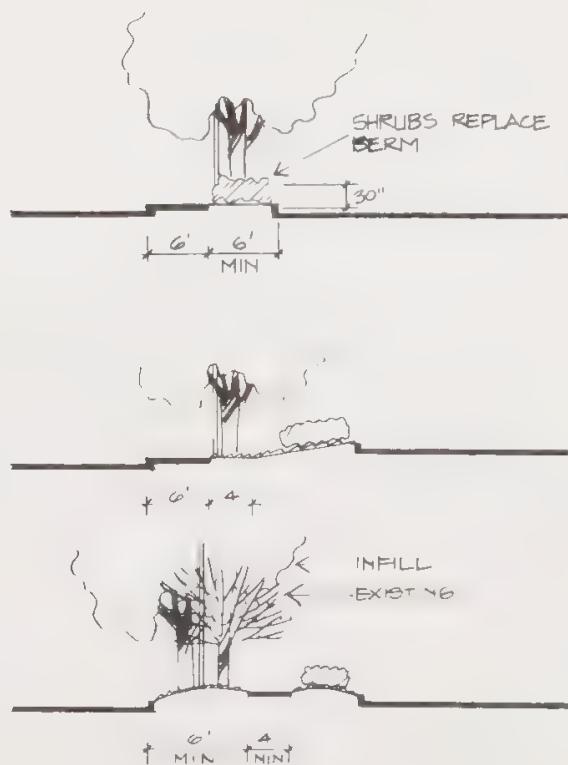
A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Existing condition does not allow space for berm. Shrub screens parking.

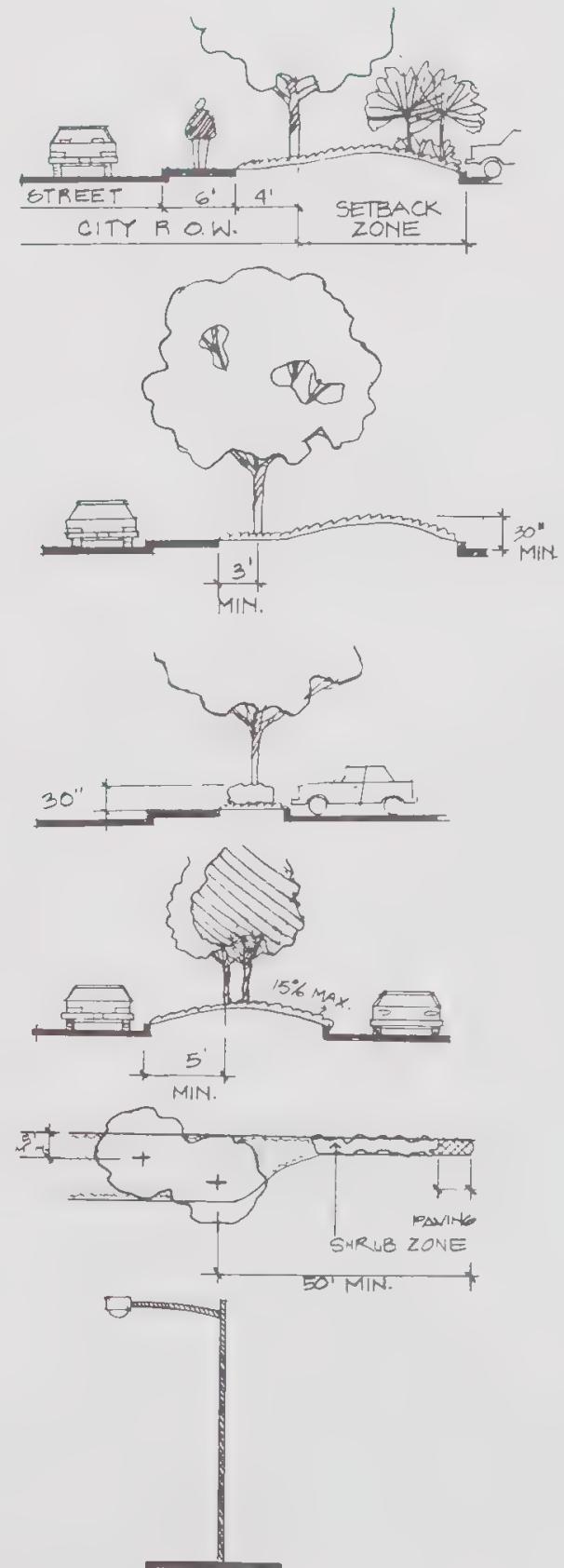
Add shrubs to existing landscape to screen parking.

Existing walk meanders. Infill trees where spacing is greater than 50'. Add shrubs to screen parking.



SPECIFIC CRITERIA: GALAXY WAY

<u>R.O.W.:</u>	Street and Median plus Adjacent 10'
<u>PARKWAY:</u>	10' of R.O.W. plus landscape setback zone
<u>SIDEWALK</u>	6' located at curb; 4' minimum when separated from curb
<u>TREES</u>	Deciduous canopy trees
Quercus rubra (Red Oak)	<ul style="list-style-type: none"> • Minimum - 15 gal. at 30' on center maximum spacing, 70% of total trees • 5' minimum from street curbs • 3' minimum from walks and interior curbs • Use accent trees at project entries
<u>SHRUBS</u>	30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage
<u>MEDIAN:</u>	Crown grade to 15% maximum side slopes
<u>TREES</u>	Tall evergreen tree
Eucalyptus nicholii Accent Trees	<ul style="list-style-type: none"> • 5 and 15 gallon, random spacing • 5' from face of curbs minimum
<u>SHRUBS</u>	Planting under 30" in height in noses 4' and wider; infill existing shrubs where spotty
<u>GROUNDCOVER</u>	Groundcover dominant
Myoporum parvifolium "Putah Creek"	
<u>LIGHTING:</u>	City standard
	<ul style="list-style-type: none"> • "Cobra Head" fixture • 30' galvanized steel pole
<u>PAVING:</u>	Concrete walks, concrete at median nose ends



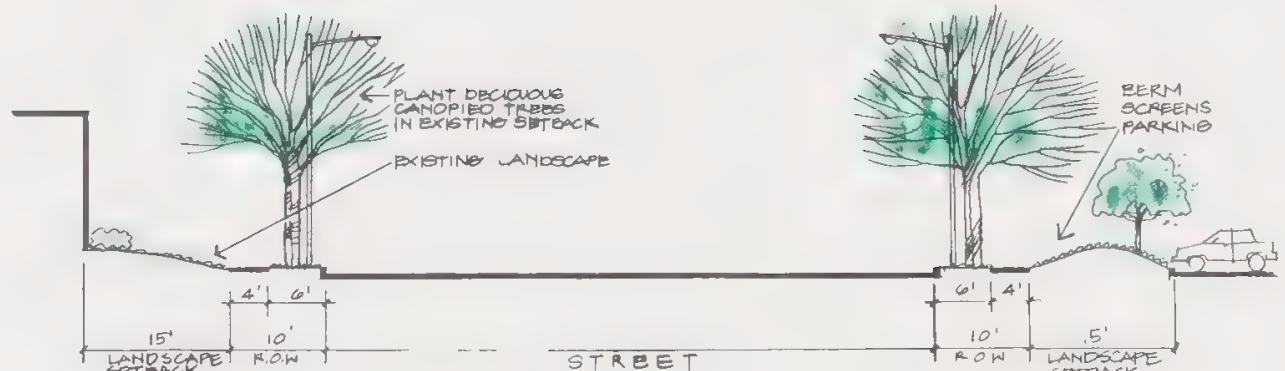
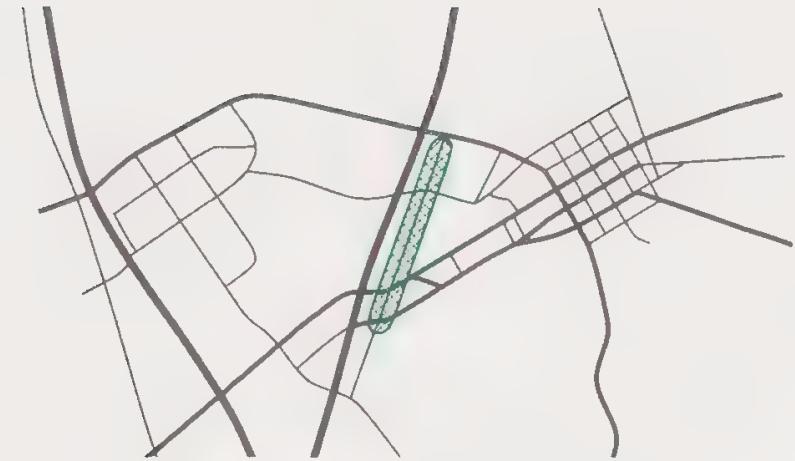
*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

III. SECONDARY STREETS: c. MARKET STREET

Market Street is presently made up of a portion of Park and Shop Center frontage and a number of smaller land uses. This character, however, will probably change as the land vacated by the railroad is redeveloped. It will function as a secondary street primarily linking downtown traffic to the various existing and proposed access points along Highway 24.

Additionally, the development of land between Highway 24 and Market Street will be of major importance in shaping the visual image of Concord due to its frontage along the entire Central Concord portion of Highway 24.

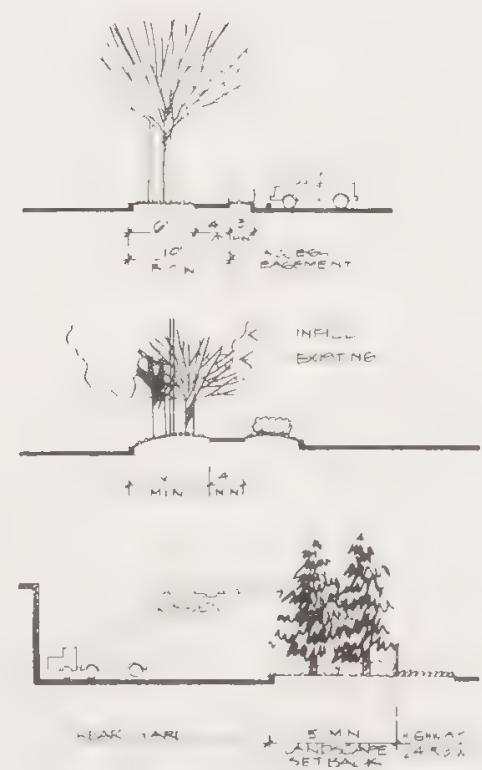
The guidelines for Market Street establish a secondary street character similar to that of Diamond Boulevard. They also propose a landscape zone at the rear property lines on all sites adjacent to Route 24.



A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Existing condition does not allow space for berm. Shrub screens parking.



Add shrubs to existing landscape to screen parking.

Rear yards along Highway 24.

SPECIFIC CRITERIA: MARKET STREET

R.O.W.:

Street and Median plus Adjacent 10'

PARKWAY:

10' of R.O.W. plus landscape setback zone

SIDEWALK

TREES

Liriodendron tulipifera
(Tulip Tree)

4' minimum separated from curb; 6' when located at curb (see prior page)

Deciduous canopy trees

- Minimum - 15 gal. at 30' on center maximum spacing; 70% of total trees
- 3' minimum from walks and interior curbs
- 5' minimum from street curb
- Use accent trees at project entries

SHRUBS

30" high to screen parking; at auto sales lot allow 20" or window equal to 40% of frontage

LIGHTING:

City standard

- "Cobra Head" fixture
- 30' galvanized steel pole

PAVING:

Concrete walks

REAR YARDS:

(Fronting Highway 24)

SETBACK

Minimum 15' landscape setback zone

TREES

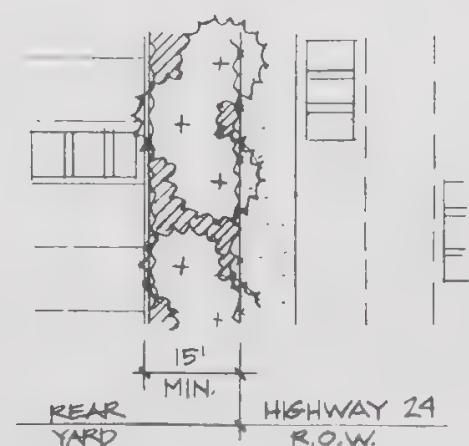
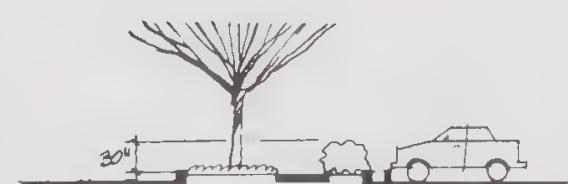
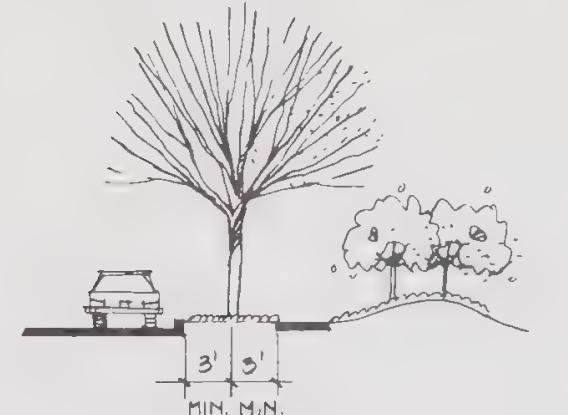
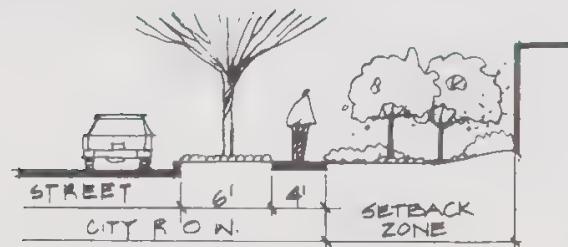
Pinus canariensis
(Canary Island Pine)
Accent Trees

Tall vertical evergreen dominant; 70% of total trees

- 3' minimum from curbs
- 15' to 30' spacing between trees

SHRUBS

3' - 5' high to screen Highway 24

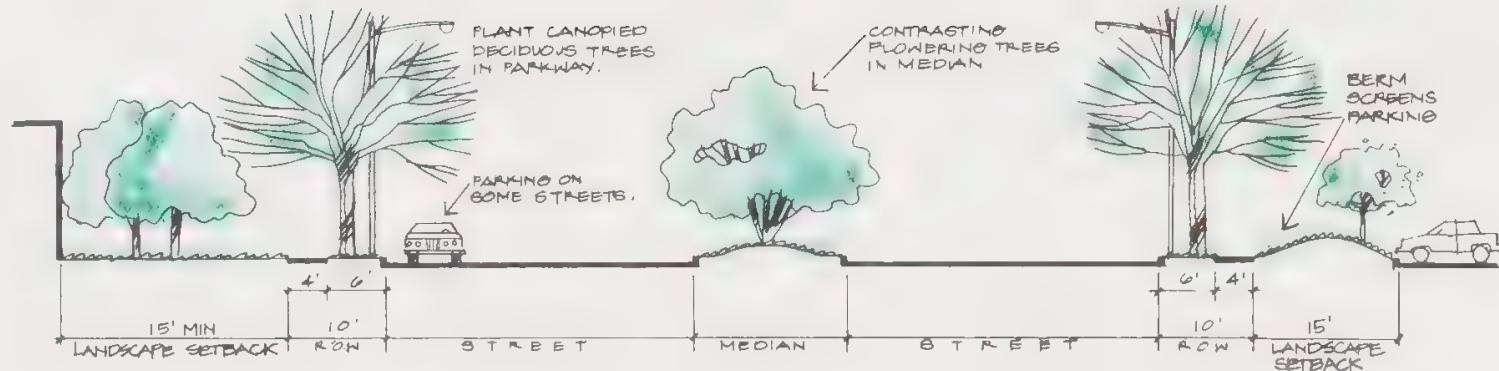
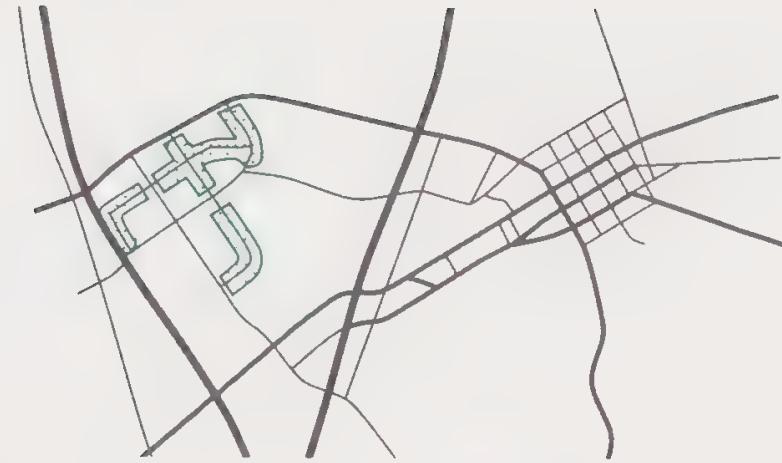


*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

IV. MERIDIAN PARK: JOHN GLENN DRIVE, BURNETT AVENUE, MERIDIAN PARK BOULEVARD AND ORION STREET

The additional Meridian Park area as defined for this study is composed of the following streets: John Glenn Drive, Burnett Avenue, Meridian Park Boulevard and Orion Street. The streets of this primarily industrial/mixed use zone are basically local streets feeding the businesses in the area. As they are not projected to the major traffic routes, the minimum landscape/sidewalk criteria is less than that of the primary image corridors.

The intent here is to retain the minimum 10' right-of-way and to establish a consistent level of quality area-wide. Within this, however, there is more variety encouraged in the selection of street trees, shrubs, walk location, etc. Here, as on Diamond Boulevard, a number of existing new projects have already set the tone with the high quality landscapes installed.



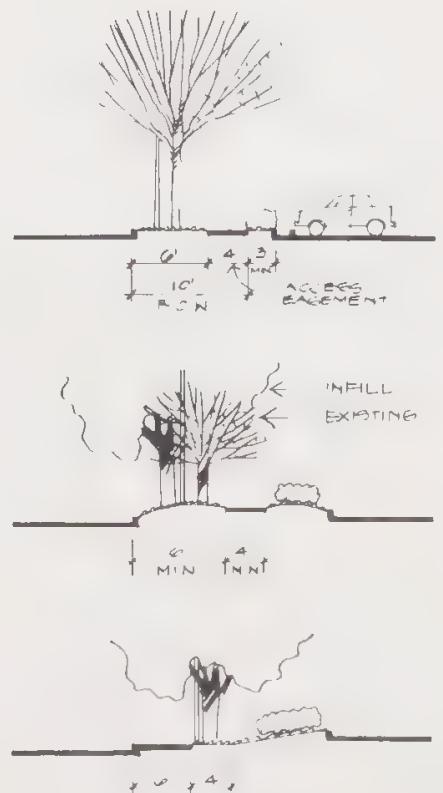
A. PREFERRED SECTIONS FOR NEW DEVELOPMENT

B. EXISTING CONDITION ALTERNATES

Existing condition does not allow space for berm. Shrub screens parking.

Walk meanders.

Existing walk is at curb with parking beyond parkway.



SPECIFIC CRITERIA: MERIDIAN PARK

R.O.W.:

Street and Median plus Adjacent 10'

PARKWAY:

10' of R.O.W. plus landscape setback zone

SIDEWALK

TREES

Gleditsia triacanthos inermis
"Shademaster"
(Shademaster Locust)

Quercus rubra
(Red Oak)

SHRUBS

4' minimum separated from curb; 6' when located at curb

Deciduous canopy trees

- Minimum - 15 gal. at 25' on center, 70% of total trees
- 3' minimum from curbs and walks
- Infill among healthy existing trees
- Use accent trees at project entries

MEDIAN:

TREES

Quercus virginiana
(Southern Live Oak)

Magnolia grandiflora "Russet"
(Magnolia)

Liquidambar styraciflua
(Sweet Gum)

SHRUBS

Crown grade to 15% maximum side slopes

Evergreen canopy tree, deciduous accent

- 5 and 15 gallon, random spacing
- 5' from face of curbs minimum

GROUNDCOVER

Trachelospermum jasminoides
(Star Jasmine)

LIGHTING:

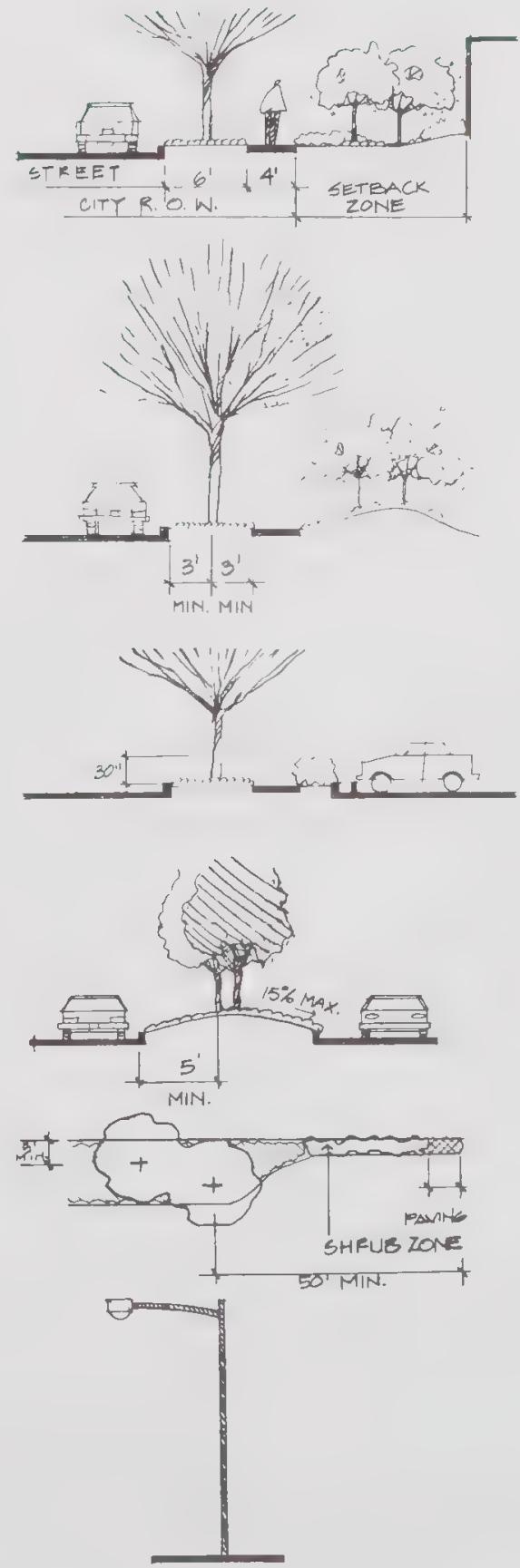
Planting in noses 5' and wider where paving not required for crosswalk; infill existing shrubs where spotty

Groundcover dominant

PAVING:

Concrete walks, concrete at median nose ends

*See pages 44 - 49 for specific details and plant lists when not otherwise identified.

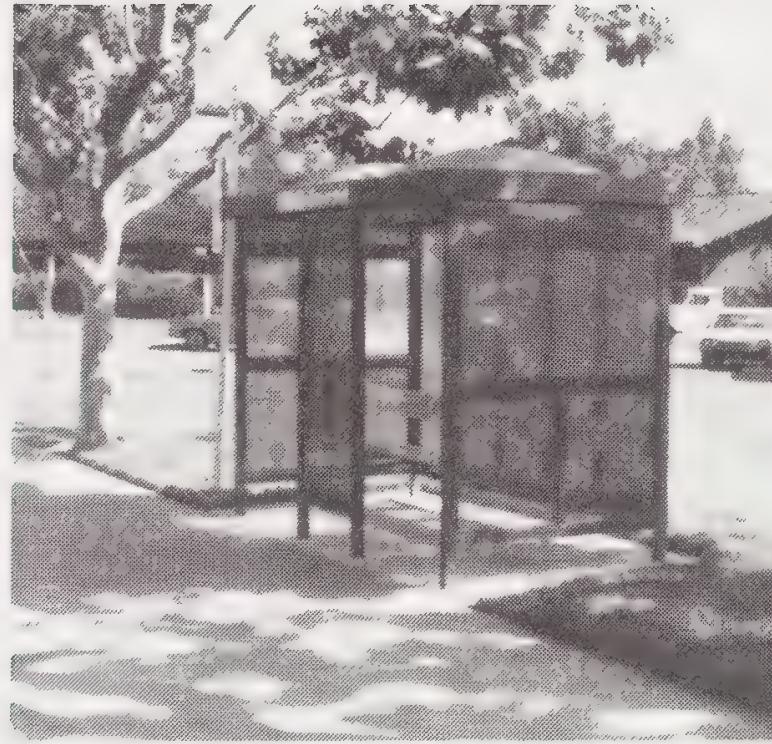


STREETSCAPE ELEMENTS: FURNISHINGS/PAVING

In the downtown core, a vocabulary of street-scape furnishings has already been established along Willow Pass Road and should be continued within this zone and along the extension of downtown. In other areas, where no furnishings are described, those selected should be handsome, durable and require relatively little maintenance.

Bus waiting shelters should be provided at major bus stops. The standard bus shelter is a simple metal and glass structure, as is to be found at the Civic Center complex. Metal should be painted or anodized to a black color. Shelters should be located behind the back of walk so as not to obstruct walking area.

Directional signage in the city does not seem to be a problem requiring development of

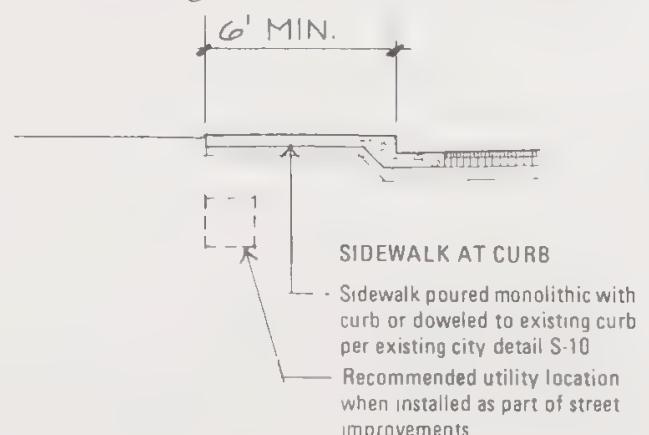
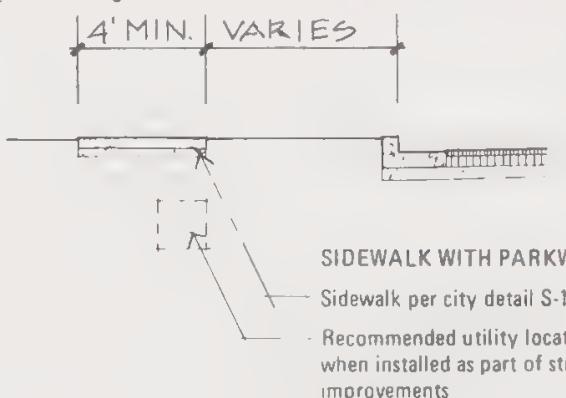


special signage systems. City entry signs however, should be consistent, low or "ground hugging" in character and employing the standard City typeface and design show below. Within this context, variety in the sign design is encouraged (i.e., concrete wall with raised painted logo, wood two or three sided sign, etc.)

Sidewalks are to be constructed per City standards, as illustrated below. Exceptions would be at building entry plazas, where a more elaborate treatment may be appropriate. In such a case, paving may extend all the way to the curb, even where a parkway at curb would otherwise be specified. An equivalent amount of planting as would otherwise be required in the parkway area must still be installed in tree

wells, planters, etc. In all cases walks should transition smoothly to meet sidewalks of adjacent properties. In addition, handicap ramps should be constructed to meet state and local requirements.

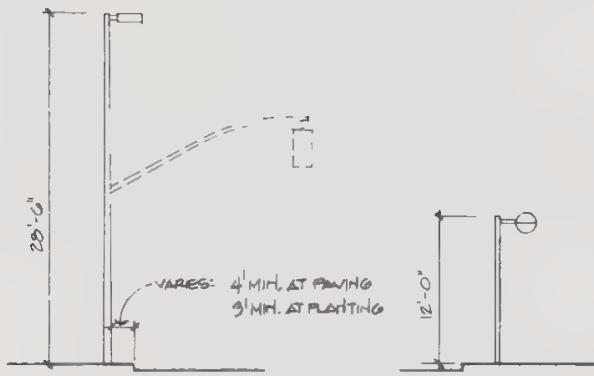
Tree wells should be a minimum of 48" square. They may be paved where appropriate with a dry-set paver, covered with a metal or concrete tree grate, or planted. The openings around tree in paving or tree grates must be expandable to allow for increase in trunk caliper of tree. Preferred materials for tree well paving would be soft-edged rectangular pavers, such as street cobbles set in sand, since these will give a relatively tight fit, while small irregularities in the surface due to the tree growth will not be obvious.



City of Concord

STREETSCAPE ELEMENTS: LIGHTING/MEDIANS

Streetlighting is of four major types as illustrated.

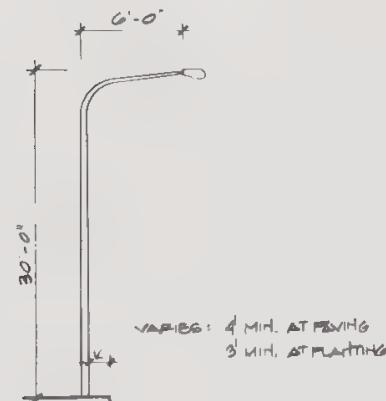


DOWNTOWN

Round cutoff fixture on tapered steel pole painted to match Todos Santos fixtures.

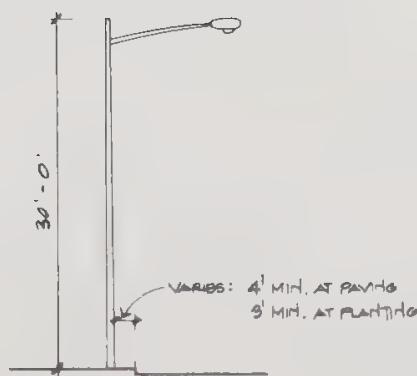
Integrate with traffic signals where occur.

Pedestrian scale fixture occurs in downtown core only. Excluding Willow Pass Road.



CROSSTOWN STREETS AND DOWNTOWN LOOP

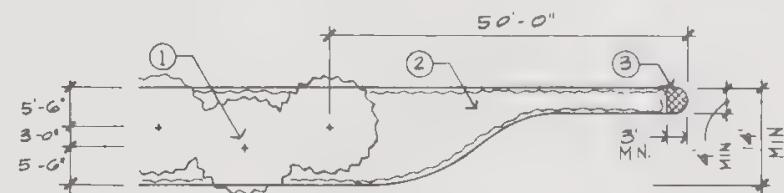
Cutoff type "Cobra Head" fixture on 30' galvanized steel pole with davit arm.



OTHER STREETS

Standard "Cobra Head" fixture on galvanized steel pole.

Medians should be planted with low maintenance shrubs, groundcovers and trees. In order to accommodate trees, medians should be constructed to a 14' minimum in width with noses 4' minimum in width. Where a median is less than 4 feet or where pedestrian access is required, a concrete exposed aggregate paving should be used. At crosswalk locations, paving should be standard concrete sidewalk paving.



- ① Trees which attain 4" or greater diameter; four feet above ground within 10 years
- ② Shrub/groundcover planting
- ③ Concrete nose at crosswalks



STREETSCAPE ELEMENTS: PLANTING CRITERIA

There are common details and criteria regarding planting that apply to all zones within this study. The landscape architects as well as City reviewers should consider the following when developing or reviewing plans.

1. Tree Selection:

While the dominant trees for each street have been specified on the previous pages, accent planting to be selected from the Plant List for Accent Trees and Ground Plane planting (page 48) is encouraged in feature locations and at individual driveway entrances. Individual tree types have differing characteristics, including sun, soils and water requirements, root structure, branching habits, litter, etc., that should be researched before using in specific site situation.

2. Irrigation:

At the time of this report the attitude of the City is that the medians and right-of-way shall be watered by automatic irrigation systems. Water should be applied in moderate quantities and be directed so as not to conflict with pedestrian or automobile movement.

3. Underground Utilities:

These should be located before walk and tree locations are finalized. Specific situations may require variances in the specified typical dimensions in order to provide root space to allow for street tree planting.

It is also important when utilities are being planned and constructed that they be located to be consistent with proposed streetscape sections.

4. Adequate Space:

Trees should be selected and planted so that there is adequate space for their growth. This includes:

a) Root Space - Generally the size canopy a tree will develop is equal to the size of its root system. To avoid problems with buckling pavement, trees should not be planted closer than three feet to walks (five feet if the roots are particularly invasive) and in some cases root barriers may need to be employed.

b) Vertical Clearance - Tree branching should not conflict with traffic and

generally must be trimmed to a 14' clearance over most streets.

c) Trunk Space - Trunks should have room to grow too, particularly in tree well conditons. Trees planted in turf should allow adequate space for a mower to clear the trunk.

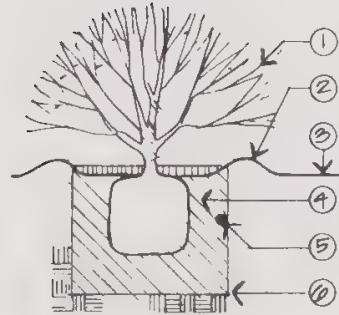
5. Sight Clearances:

Plant materials should be located and pruned so as not to obstruct traffic signs or sight lines at intersections and driveways. Additionally, trees should be held a minimum of 25 feet from street lights and 50 feet from the ends of median islands. Shrubs in median noses are to be a variety easily maintained at a maximum 30' height.

6. The Plant List for Accent Trees and Ground Plan Planting (page 48) consists of the recommended palette from which accent trees and other plants not listed in the specific criteria are to be selected.

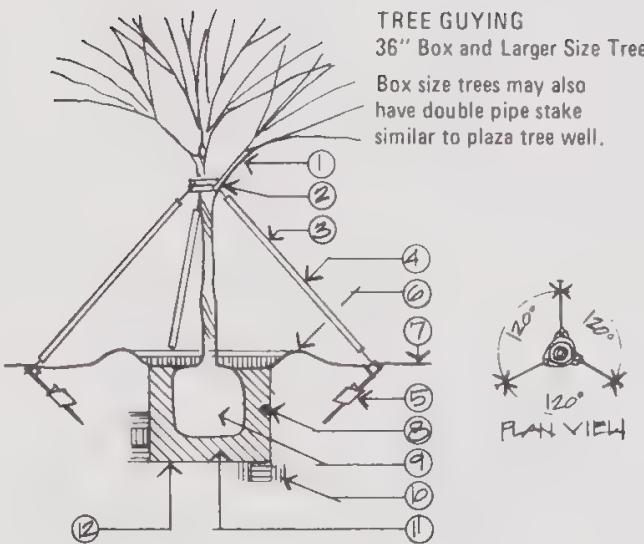
7. Planting and Staking:

The following details or their equivalent should be used for all street planting:



SHRUB PLANTING

- 1) 5 gallon shrub.
- 2) Water Basin - 3" minimum depth with 2" depth mulch.
- 3) Finish grade.
- 4) Amended backfill.
- 5) Slow release fertilizer tablet.
- 6) Scarify sides of planting pit and break up bottom soil to a depth of 1'

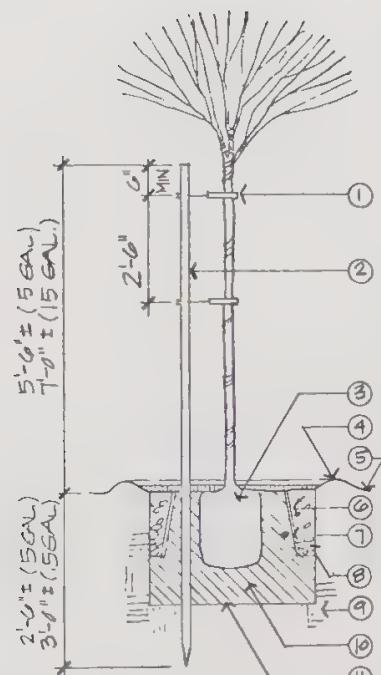


TREE GUYING

36" Box and Larger Size Trees
Box size trees may also have double pipe stake similar to plaza tree well.

- 1) Box tree.
- 2) Reinforced rubber hose tie with 9 gauge wire.
- 3) Guy as required to support tree (3 minimum), additional guys as directed by landscape architect.
- 4) 1/2" P.V.C. sleeve in lawn only.
- 5) 3/4" diameter x 36" long steel vane guy anchor.
- 6) Water basin - 3" minimum depth, remove in lawn areas after initial watering. Provide 2" depth mulch in basin.
- 7) Finish grade.
- 8) Slow release fertilizer tablet.
- 9) Root ball.
- 10) Existing soil.
- 11) Amended backfill.
- 12) Scarify sides of planting bed and break up bottom soil to a depth of 1'.

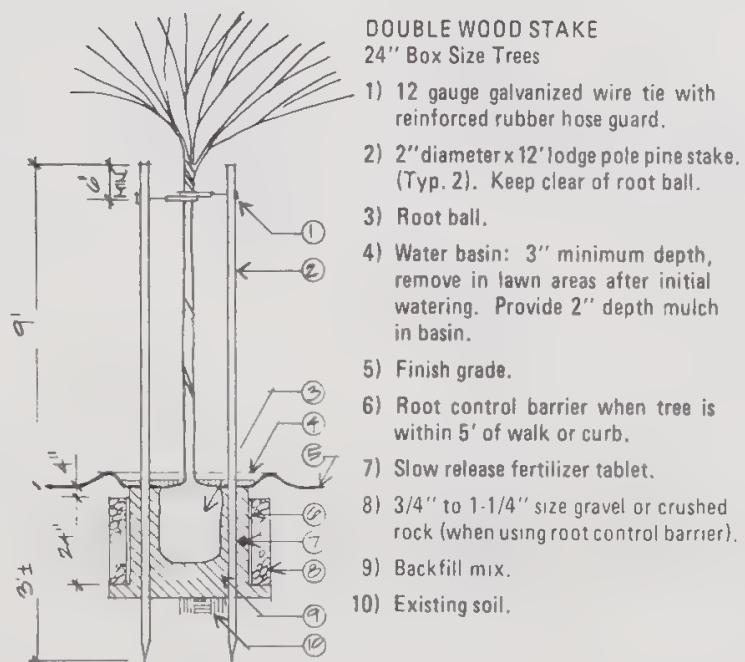
NOTE: Provide root barrier similar to double wood stake detail where planting within 5' of walk or curb.



SINGLE WOOD STAKE

5 and 15 Gallon Trees

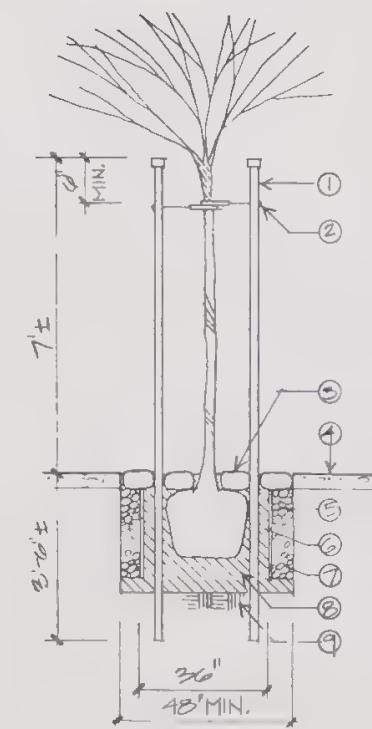
- 1) 12 gauge galvanized wire tie with reinforced rubber hose guard.
- 2) 2" diameter x 10' (8' for 5 gal.) lodge pole pine stake. Keep clear of root ball.
- 3) Root ball.
- 4) Water basin: 3" minimum depth, remove in lawn areas after initial watering. Provide 2" depth mulch in basin.
- 5) Finish grade.
- 6) Root control barrier when tree is within 5' of walk or curb.
- 7) Slow release fertilizer tablet.
- 8) 3/4" to 1 1/4" size gravel or crushed rock (when using root control barrier).
- 9) Existing soil.
- 10) Amended backfill.
- 11) Scarify sides of planting bed and break up bottom soil to a depth of 1'.



DOUBLE WOOD STAKE

24" Box Size Trees

- 1) 12 gauge galvanized wire tie with reinforced rubber hose guard.
- 2) 2" diameter x 12' lodge pole pine stake. (Typ. 2). Keep clear of root ball.
- 3) Root ball.
- 4) Water basin: 3" minimum depth, remove in lawn areas after initial watering. Provide 2" depth mulch in basin.
- 5) Finish grade.
- 6) Root control barrier when tree is within 5' of walk or curb.
- 7) Slow release fertilizer tablet.
- 8) 3/4" to 1 1/4" size gravel or crushed rock (when using root control barrier).
- 9) Backfill mix.
- 10) Existing soil.



PLAZA TREE WELL

- 1) 1-1/2" diameter black pipe. End to be threaded, capped, and painted to match pipe. Keep clear of root ball.
- 2) 12 gauge galvanized wire tie with reinforced rubber hose guard.
- 3) Cobble or brick on sand, or tree grate.
- 4) Adjoining paving.
- 5) 24" deep root barrier. "Deep Root" UBP - 24 or approved equal (714/898-0563).
- 6) Slow release fertilizer tablet.
- 7) 3/4" to 1-1/4" size gravel or crushed rock.
- 8) Amended backfill mix.
- 9) Existing soil.

NOTE: Detail is based on 24" box size tree. For larger sizes, planting pit and root barrier must be enlarged accordingly.

PLANT LIST FOR ACCENT TREES AND GROUND PLANE PLANTING

ACCENT TREES	COMMON NAME	SIZE	GROWTH RATE	EG/ DECID:	DES. SIZE	SHOWY SEASON	COMMENTS
<i>Acer buergerianum</i>	(Trident Maple)	24" Box	F	D	15 - 20'	Autumn	Red, orange, yellow
<i>Aesculus carnea "briotii"</i>	(Red Horsechestnut)	24" Box	S	D	20'	Apr - May	Soft pink/red plumes
<i>Albizia julibrissin</i>	(Silktree)	15 Gal/24" box	F	D/SE	30'	Summer	Pink fluffy flower
<i>Cercis occidentalis</i>	(Western Redbud)	24" Box		D	12 - 15'	Yr. Rnd.	Spring - Magenta flower; Fall - Red
<i>Crataegus phaenopyrum</i>	(Washington Thorn)	24" Box	F - M	D	25'+	Fall & Wntr	Orange/red leaves follow berries
<i>Eucalyptus amygdalina angustifolia</i>	(Peppermint Eucalyptus)	15 Gal	F	EG	30'		Weeping graceful form
<i>Geijera parviflora</i>	(Australian Willow)	15 Gal	M	EG	25'		Blue-green grey color, weeping form
<i>Ginkgo biloba "Autumn Glory/Gold"</i>	(Maidenhair Tree)	24" Box/36" Box	S	D	30'	Fall	Golden yellow, long-lived, male
<i>Gleditsia triacanthos "Shademaster"</i>	(Honey Locust)	15 Gal	F	D	30 - 40'	Fall	Golden yellow
<i>Koelreuteria bipinata</i>	(Chinese Flame Tree)	15 Gal	S - M	D	25 - 35'	Sumr & Fall	Small yellow flower, orange/red caps
<i>Liquidambar styraciflua</i>	(Sweet Gum)	15 Gal/24" Box		D	40'	Fall	Red to bronze
<i>Magnolia grandiflora</i>	(Southern Magnolia)	24" Box		EG	30'	Yr. Rnd.	White flowers
<i>Malus purpurea "Eleyi"</i>		24" Box		D	20'	Late Apr	Wine red and deep pink flowers, fruit, purple/bronze leaves
<i>Melaleuca linarifolia</i>	(Flaxleaf Paperbark)	15 Gal/24" Box	M	EG	20'	Summer	White flowers, papery white bark
<i>Nyssa sylvatica</i>	(Sour Gum, Tupelo)	15 Gal/24" Box	S - M	D	30'	Fall	Brilliant red, orange, yellow
<i>Olea europaea</i>	(European Olive)	15 Gal/24" Box	S	EG	20 - 25'		Grey green foliage
<i>Phoenix canariensis</i>	(Canary Island Date Palm)	10' Min. Height		EG	20 - 40'		
<i>Phoenix dactylifera</i>	(Date Palm)	24" Box		EG	40 - 60'		
<i>Pinus canariensis</i>	(Canary Island Pine)	15 Gal/24" Box	F	EG	50 - 60'		
<i>Pistacia chinensis</i>	(Pistache)	15 Gal/24" Box	S - M	D	30'	Fall	Orange & red, male, litter, red bark
<i>Populus nigra "Italica"</i>	(Lombardy Poplar)	15 Gal	F	D	40 - 50'	Fall	Yellow
<i>Prunus cerasifera "Thundercloud"</i>	(Flowering Plum)	15 Gal/24" Box		D	30'	Feb - Mar	White, pink flowers, purple leaves, little
<i>Pyrus calleryana "Bradford" "Aristocrat"</i>	(Ornamental Pear)	24" Box		D	30'	Fall	White flowers
<i>Quercus coccinea</i>	(Scarlet Oak)	24" Box	S - M	D	30'	Fall	Bright red
<i>Robinia idahoensis</i>	(Pink Locust)	15 Gal/24" Box	F	D	40'	April	Deep purple/pink flowers, aggressive roots
<i>Tilia tomentosa</i>	(Silver Linden)	24" Box	S - M	D	30'	June - July	Fragrant yellow flowers/silver leaves
<i>Tipuana tipu</i>	(Tipu Tree)	15 Gal		D/SE	25'	June - July	Apricot flower; wind is a problem

GROUNDCOVER

COMMON NAME	SPACING/SIZE
(CapeWeed)	12 - 18"/Flats
(Winter Blooming Bergenia)	2'/1 Gal
(Carmel Creeper)	2'/1 Gal
	2'/1 Gal
	2 - 3'/Flats, 1 Gal
	12"/Flats
(English Ivy)	12"/Flats
	2'/1 Gal
(Aaron's Beard)	12 - 15"/Flats
	2'/1 Gal
	1 Gal/Flats
	Seed
(Japanese Honeysuckle)	2 - 3'/Flats
	2'/1 Gal
	2-3'/1 Gal
(Mondo Grass)	6 - 8"/Flats
(Trailing African Daisy)	12"/Flats
	12"/Flats
(Spring Cinquefoil)	12"/Flats
(Trailing Rosemary)	2'/Flats/1 Gal
(Star Jasmine)	18"/1 Gal
(Dwarf Periwinkle)	12"/Flats

SHRUBS - Screen & Accent (2 - 5')

<i>Abelia grandiflora "Edward Goucher"</i>
<i>Acanthus mollis</i>
<i>Agapanthus</i>
<i>Berberis species</i>
<i>Buxus microphylla japonica</i>
<i>Ceanothus species</i>
<i>Chaenomeles</i>
<i>Choisya ternata</i>
<i>Cotoneaster</i>
<i>Dieteria vegetata</i>
<i>Escallonia species</i>
<i>Felecia amelloides</i>
<i>Grevillea "Noellii"</i>
<i>Hebe species</i>
<i>Juniperus species</i>
<i>Lavandula angustifolia</i>
<i>Ligustrum j. "Texanum"</i>
<i>Mahonia aquifolium "Compacta"</i>
<i>Myrtus communis "Compacta"</i>
<i>Nandina domestica "Compacta"</i>
<i>Nerium oleander "Petite var."</i>
<i>Spiraea species</i>
<i>Viburnum davidii</i>

COMMON NAME

COMMON NAME	SPACING/SIZE
	3'/5 Gal
	2'/1 Gal
(Lily of the Nile)	2'/1 Gal
(Barberry)	3'/5 Gal
(Japanese Boxwood)	3'/5 Gal
	3'/5 Gal
(Flowering Quince)	3'/5 Gal
(Mexican Orange)	3'/5 Gal
	3'/5 Gal
	2'/5 Gal
	3'/5 Gal
	18"/1 Gal
	4'/5 Gal
	3'/5 Gal
(Juniper)	3'/5 Gal
	18"/1 Gal
(Japanese Privet)	3 - 4'/5 Gal
	3'/5 Gal
	2'/5 Gal
(Heavenly Bamboo)	2 - 3'/1 - 5 Gal
	3'/5 Gal
	3'/5 Gal
	1 - 2'/1 - 5 Gal

STREETSCAPE ELEMENTS: MAINTENANCE

The City of Concord has several options for funding streetscape maintenance. Given limited City budgets, it is desirable to spread these costs among the property owners who will benefit from these improvements. These are discussed further in the implementation section of this Plan.

Whatever funding option is selected, it is important to have some well defined maintenance criteria, particularly if the work is done by a number of different parties.

Criteria should include not only such things as watering and fertilization to protect the health of the plantings and protect the City's investment, but also a discussion of desired landscape character. This would include whatever pruning techniques are appropriate to achieve the desired

effect; replacement, addition or removal of plant materials, seasonal color at feature areas, etc.

Pruning of trees should be performed to maintain the health of the tree and to avoid conflicts with pedestrians and vehicles. Trees near the curb should be pruned to encourage an upright branching structure rather than removing all branches up to a specified height.

Shrubs should be loosely pruned to maintain their shape and size, but should not be clipped except where the design specifically calls for a formal hedge.

A maintenance specification setting forth such criteria which is under review by the City at the time of this writing is included in the appendix.

IMPLEMENTATION

IMPLEMENTATION

The implementation of this Street Beautification Plan affects both areas within the public right-of-way and on private property adjacent to the public right-of-way. As a result, there are two different methods by which this plan will be implemented. These are described in the text that follows. In addition, the approval processes for both public and private projects are summarized in the two charts included in this section.

Implementation within the Public Right-of-Way

Implementation of this plan within the public right-of-way involves reconstruction on the major arterials in the downtown that are the subject of this plan. As set forth on page 9 of this plan, virtually all of the arterials subject to the plan are scheduled for reconstruction or improvement in the future to add additional capacity.

This provides both a constraint and an opportunity for the implementation in the right-of-way area. It is a constraint to the extent that significant landscape improvements should not be made to existing arterials scheduled for substantial reconstruction that would require destruction of the new planting. It presents an opportunity in that the new landscape section dimensions can be incorporated into street widenings as they occur in the downtown area.

Pursuant to existing City policy, the area between the curb and the property line (sidewalk and/or parkway area) will be the responsibility of the adjacent property owner for installation and ongoing maintenance. Incremental installation will therefore take place as development or redevelopment occurs on parcels adjacent to the right-of-way.

It is the intent of the Agency and City to implement this plan in the public right-of-way in median areas as street reconstruction occurs (with the exception of an initial demonstration project to occur as part of the formation of the proposed Downtown Landscape Assessment District). As these street improvement projects are designed, this plan will be used as a guideline for City staff to develop the appropriate sections and landscape plans for the street improvement projects.

The implementation of the plan within the public right-of-way area could be funded by a variety of sources. It is likely that the sources of funds to improve the street system capacity will be the same sources of funds used to install landscaping in conformance with this plan. Potential funding sources include state and federal funds, Redevelopment Agency funds, and funds that could be provided by the formation of Capital Improvement Assessment Districts.

PUBLIC DEVELOPMENT – APPROVAL PROCESS

- I. Engineering division or consultant drafts preliminary street and landscape plans incorporating:
 - Street geometrics and dimensions recommended by traffic engineers and this Central Concord Redevelopment Area Street Beautification Plan
 - City Circulation Element
 - Landscape requirements of this plan
- II. Plans are reviewed by Engineering, Planning, Redevelopment and Parks Staff.
- III. Revised plans are submitted to City Council for review. (Also Redevelopment Agency if redevelopment funding is involved.)
- IV. Engineering Division or consultant completes construction drawings.
- V. Project is let out to bid.
- VI. City Council awards contract.

The exact method of financing, and the amount required will have to be developed at such time as specific street improvement projects are proposed for construction. It is not possible at this time to specify the exact funding source or method of implementation without having a specific street improvement project proposed.

Implementation of the landscape improvements called for in this plan also implies that the improvements will be properly maintained after they are installed. This requires a strong commitment by the City to a maintenance standard higher than existing policy. Due to budget constraints, it is necessary that the cost of this higher maintenance standard be born by the property owners benefiting from the landscape improvements. Implementation of this plan in the public right-of-way is therefore contingent on the creation and ongoing renewal of a Downtown Landscape Maintenance District or some other means of continued funding.

Implementation on Private Property

Most of the sections in this report show landscaping on private property behind the right-of-way line that is coordinated with the landscaping of the public right-of-way. Since much of the existing negative appearance of arterials in Concord results from the lack of landscaping on private property, implementation of this portion of the plan is crucial to its overall success.

It is the intent of this plan that the portions affecting the adjacent private properties will be

PRIVATE DEVELOPMENT – APPROVAL PROCESS

- I. Pick up pertinent materials from Redevelopment Agency and/or Planning Department including:
 - Application for Use Permit
 - Current Zoning
 - Parking Requirements
 - Copy of the Redevelopment Plan
 - Copy of this Central Concord Redevelopment Area Street Beautification Plan
- II. Submit preliminary plans for City staff review and comment including:
 - Site Plan
 - Building Elevations
- III. Optional Step: Submit plans for informal review by Design Review Board.
- IV. Apply to Planning Commission for Use Approval including:
 - Revised Site Plan and Elevations
 - Approval Fees
- V. Apply to Design Review Board for Architectural Approval.
 - Revised Site Plan and Elevations
 - Landscape Plan
- VI. Submit Final Plans to Building Department for Plan Check and Building Permit including:
 - Construction Documents
 - Building Permit Fees
 - Development Fees (e.g., Offsite Street Improvement fee, Parkland Dedication fee, etc.)

implemented as the properties develop or redevelop. At that time, when new developments are approved by the Planning Commission and the Design Review Board, landscape setbacks will be required pursuant to existing City ordinances.

Applicants for developments within the downtown are currently required to provide landscape plans, for approval by the Design Review Board, which show a minimum landscape setback of 15 feet pursuant to the Downtown Business District Ordinance. These requirements will continue with the implementation of this plan. What will change is that the Planning Commission and the Design Review Board will use the plan as a guideline for approving landscape and site plans. Under this arrangement, it will continue to be the obligation of the property owner to pay for the installation and ongoing maintenance of the landscaping required by existing ordinance and this plan.

In addition, as previously stated, it shall be the continued policy of the city to require a landscape maintenance agreement for all future developments. These agreements provide for the private property owner to maintain the landscaping within the adjacent public right-of-way.

The Planning Department will make available copies of the plan for applicants at the time they request information for a use permit or Design Review Board application. Applicants will be requested to take into account applicable sections in their design process.

PRIORITY PROJECTS

Setting priorities for streetscape improvements will be an ongoing process, based on funds available, projected street improvements, and other new construction projects. In general, however, we would recommend a concentration of efforts, building out from the strong image of the downtown Todos Santos area, as opposed to spreading limited resources too thinly across the entire study area.

We have then identified the following as priority projects or zones for concentrations of effort, although they need not be pursued in the order listed.

1. Clean up any areas that are in a state of disrepair, decayed or unsafe condition (i.e., portion of Willow Pass Road median) as an interim measure pending future street improvements.
2. Downtown Zone: Continue Todos Santos treatment to existing streets as a part of future redevelopment efforts.
3. Develop Downtown Gateways.
4. Extend Downtown image south along Galindo Street to the Downtown Gateway on Galindo Street and west along commercial zone of Willow Pass Road.
5. Develop crosstown street corridors.
6. Develop existing and future city entries off of Highway 24 and regional entries off of Interstate 680.
7. Organize signals and signage at Willow Pass Road/Market Street intersection when interchange is relocated to Clayton Road.
8. Continue development of Diamond Boulevard, northward from existing streetscape at the Willows and Chevron Card Center.



Infill of the remaining portions of the streetscape system, such as the remainder of the Downtown Loop and the Meridian Park area, can occur as opportunities and funds become available.

APPENDIX

APPENDIX

REFERENCES

Downtown Business District Ordinance
Written by the Planning Department
Adopted by City Council July 1977

Parking Requirements Ordinance
Written by the Planning Department
Adopted by City Council November 1979

Proposed Downtown Plan
Written by ELS Design Group
Not yet completed

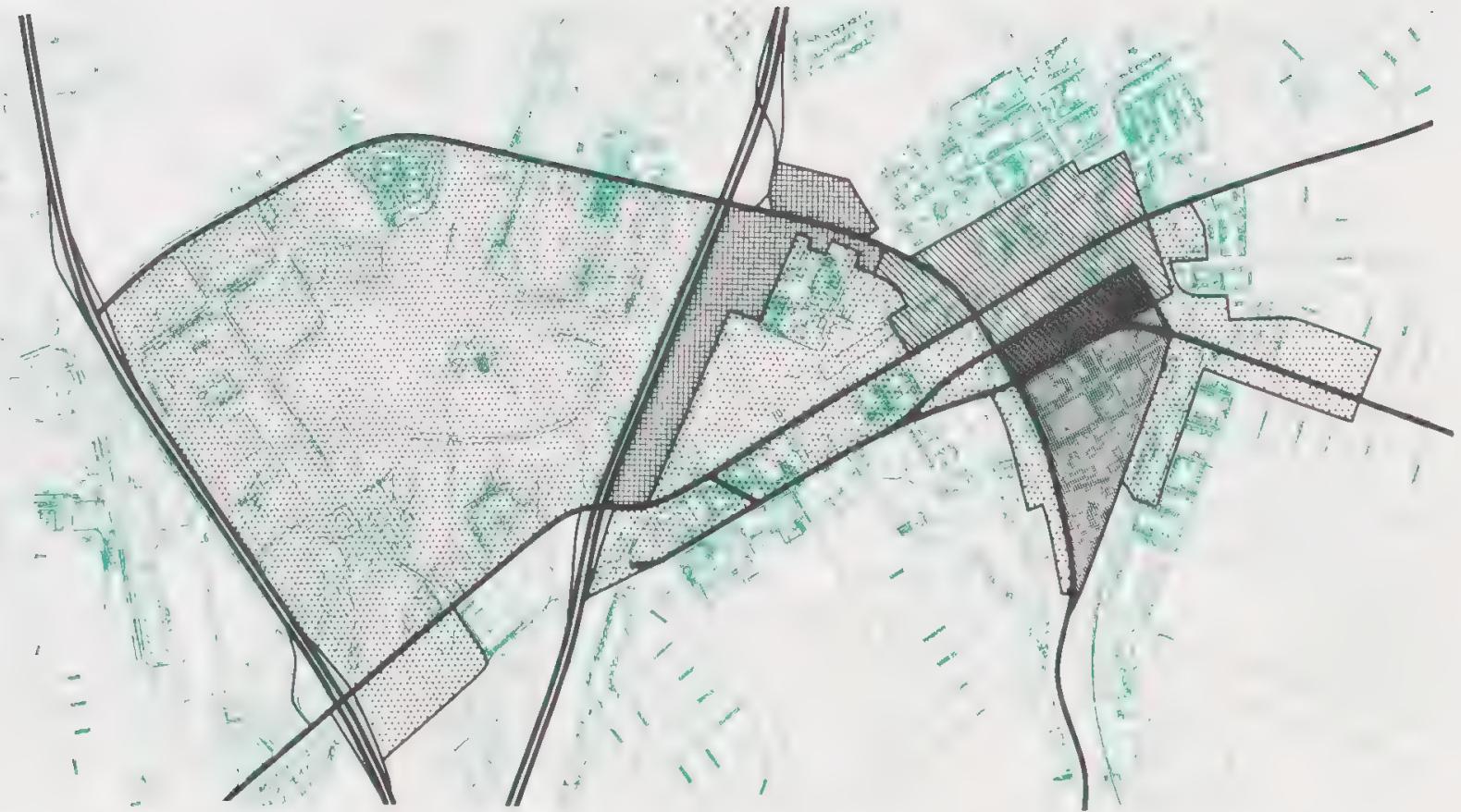
City of Concord General Plan/Land Use Element
Written by Planning Department
Adopted by City Council July 1982

City of Concord Central Concord
Redevelopment Plan as Amended
Written by the Redevelopment Agency
Amended May 1983

Master Plan for Street Trees for the
Major Thoroughfares
Written by Mai K. Arbegast
Adopted November 1979

Transportation Element
Written by the Public Works Department
Adopted by City Council August 1982

EXISTING SETBACKS



CENTRAL CONCORD REDEVELOPMENT PROJECT AREA

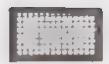
General Controls

Building: 5 FAR maximum
279 foot above mean sea level height maximum *
100 DU/acre maximum
15 foot setback for front and side exterior.

Landscape: All setbacks to be landscaped and maintained by owner.

Utilities: All underground

**Subject to TERPS study*



MARKET STREET OVERLAY DISTRICT

Setback: 15 feet for front and exterior sides, fully landscaped.



TODOS SANTOS OVERLAY DISTRICT

Plaza Frontage

Setback: No minimum
Height: 30 foot maximum at front plaza.
Thereafter 1½ foot setback per 1 foot additional height. (Maximum 100 foot height at 125 foot setback)

Landscaped Boulevard Frontage

Setback: 10 feet for building height of 15 feet or less.
Thereafter 2/3 foot setback for 1 foot additional height (30 feet maximum)

Pedestrian Activity Frontage

Setback: 10 feet maximum (except for outdoor dining)

Parking Lots

Screened to public view.



WEST BART AREA OVERLAY DISTRICT

General Controls

Building: Inner Area FAR
0.6 min.-non-residential
1.2 min.-residential
Perimeter FAR

0.4 min.-non-residential
0.8 min.-residential

Setback: None on Grant St. Possible 10 feet unless used for dining.

Parking: Screened to public view.

Clayton Road/Galindo Street/East Street

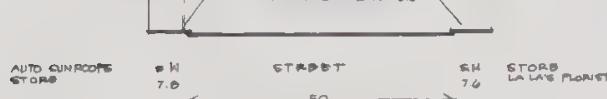
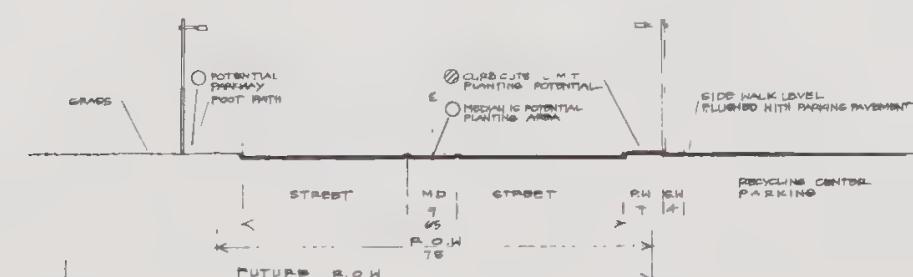
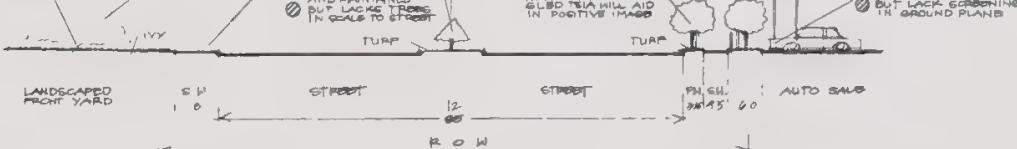
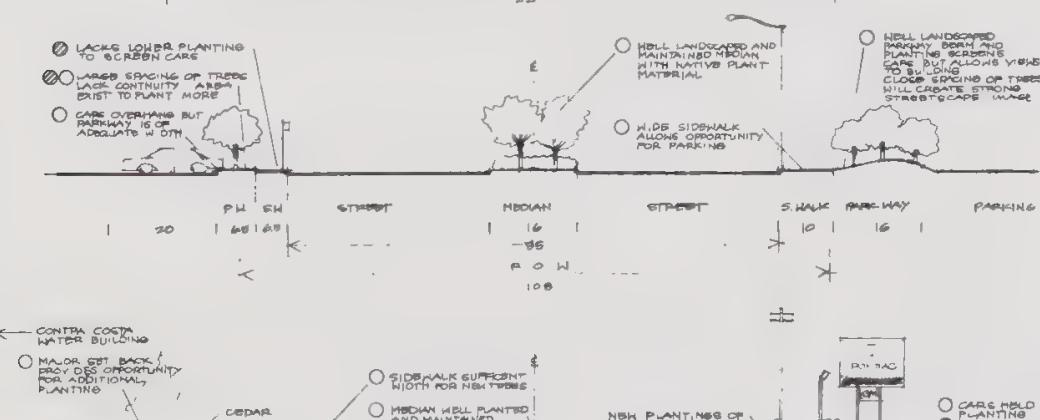
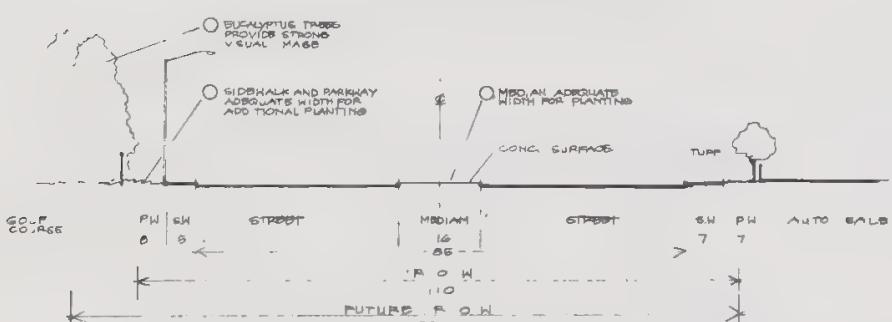
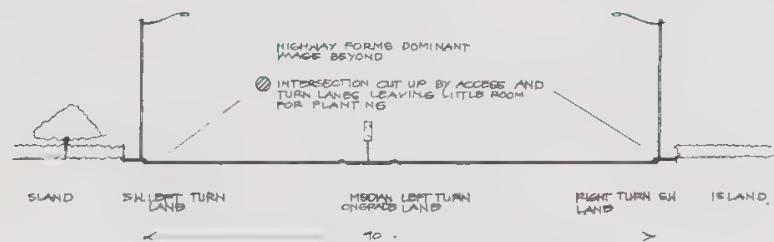
Setback: 10 feet for buildings 15 feet in height.
Thereafter, 2/3 foot setback for 1 foot additional height (30 feet maximum)

Remaining Streets

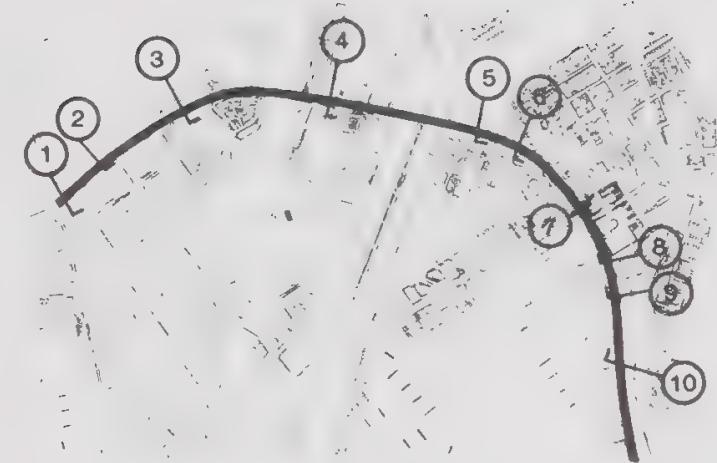
Setback: None up to buildings 30 feet in height.
Up to 30 feet maximum setback for buildings exceeding 30 feet in height.

EXISTING CONDITION SECTIONS

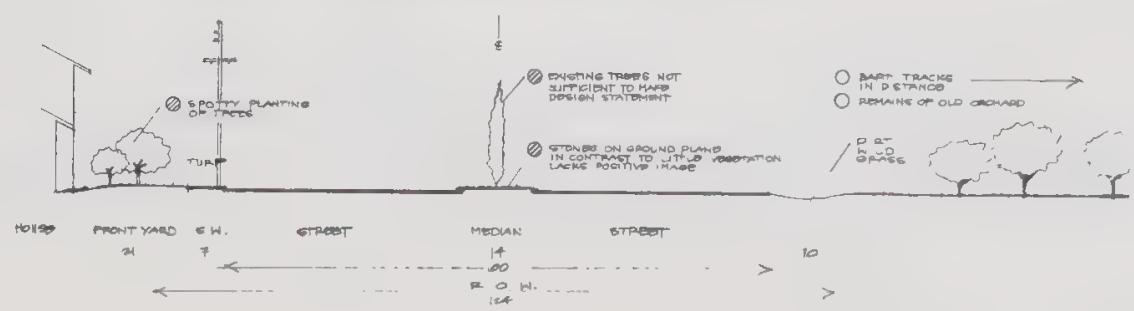
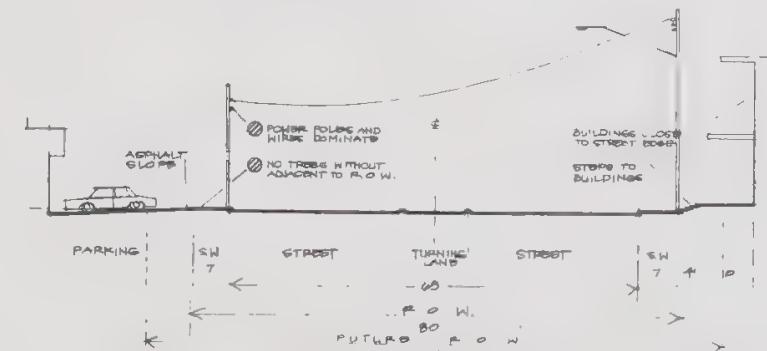
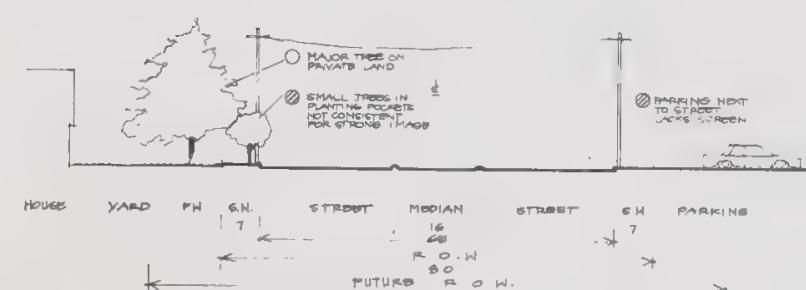
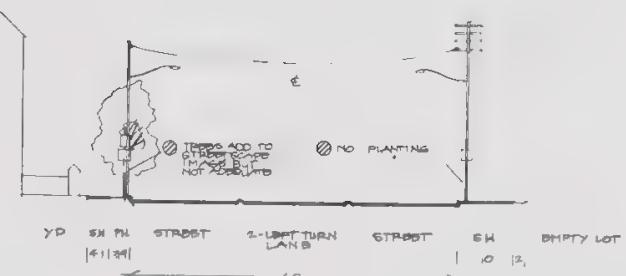
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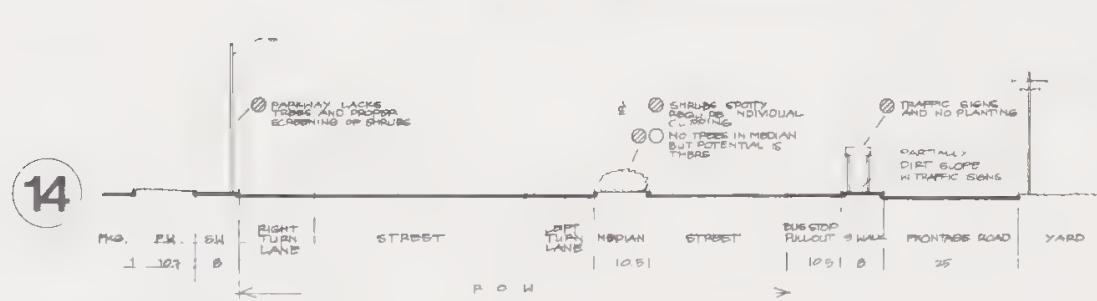
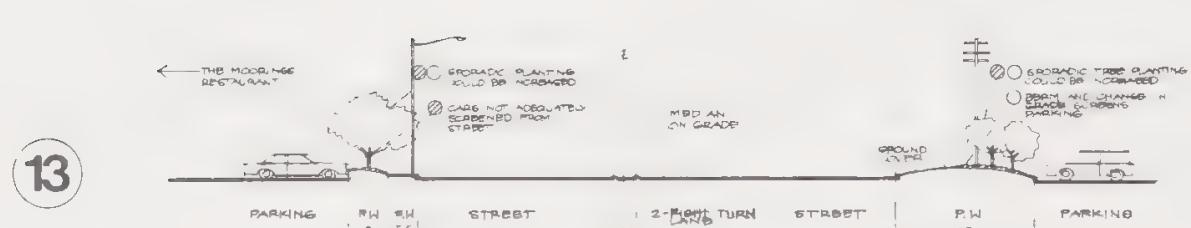
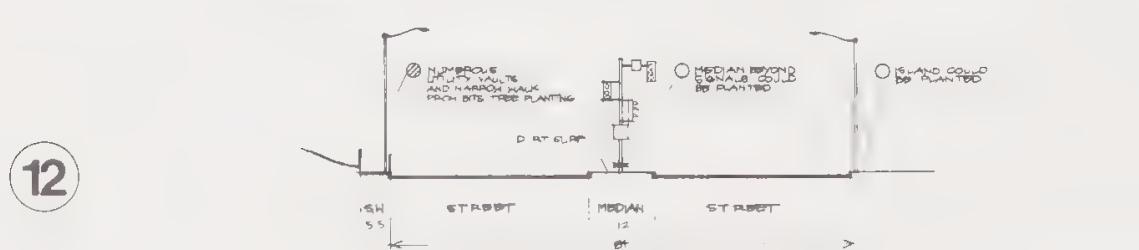
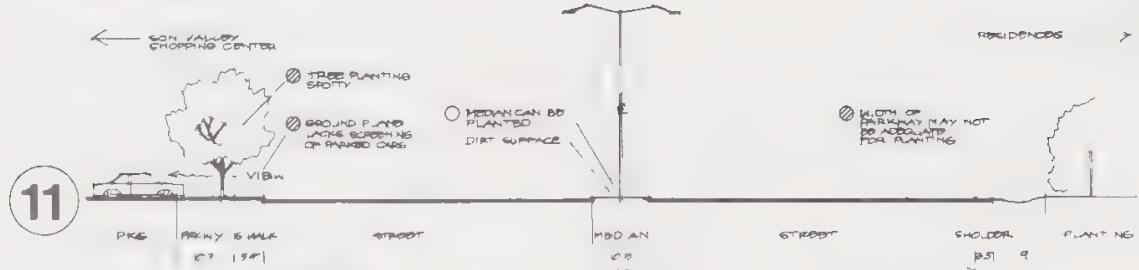
DOWNTOWN LOOP



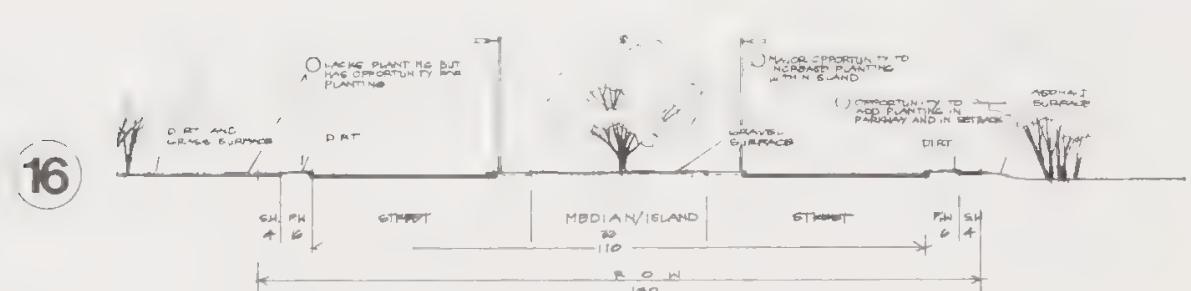
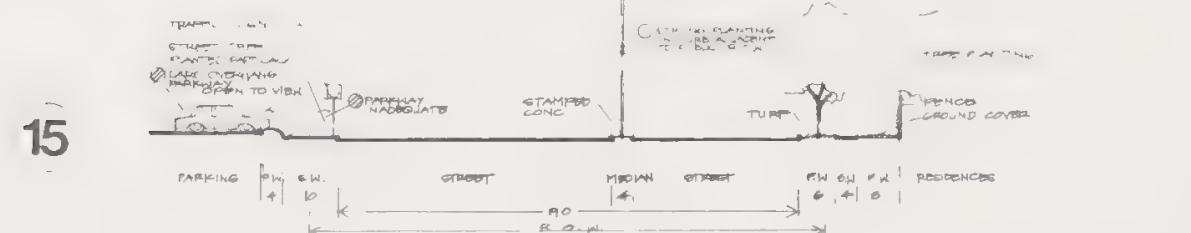
GALINDO STREET



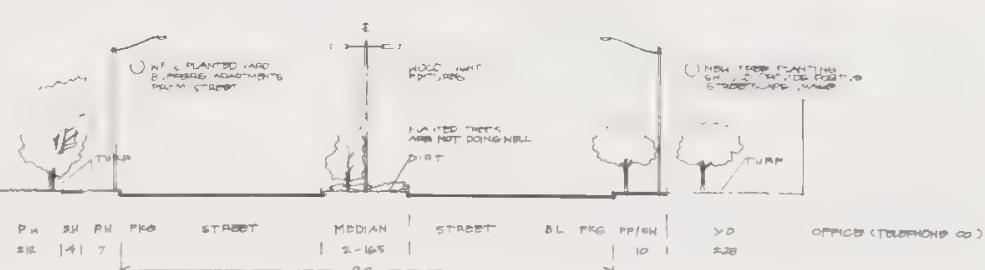
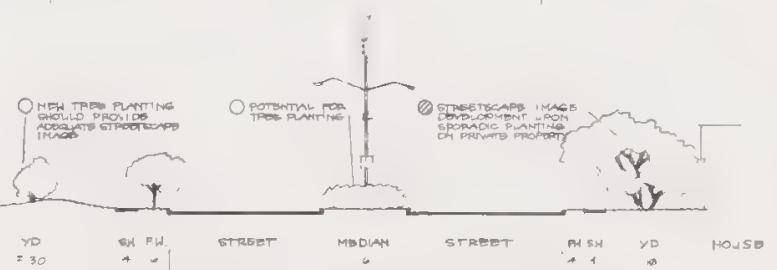
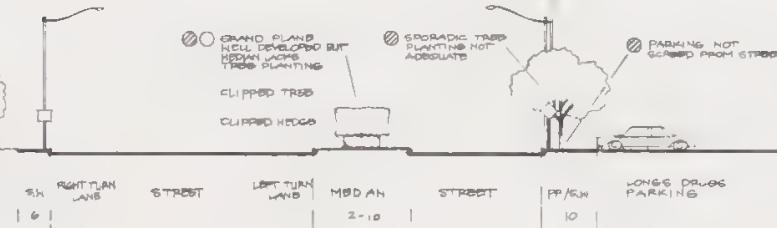
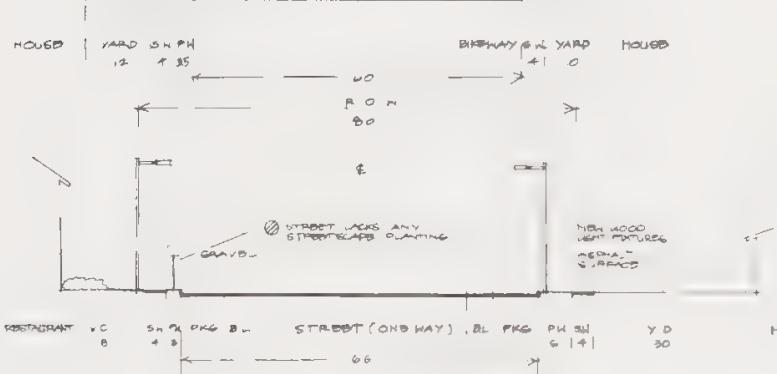
WILLOW PASS ROAD



CLAYTON ROAD

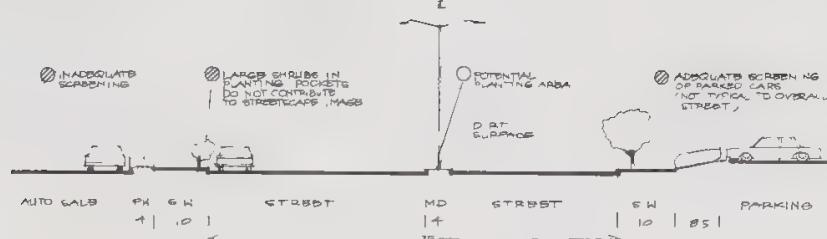


CROSSTOWN STREET

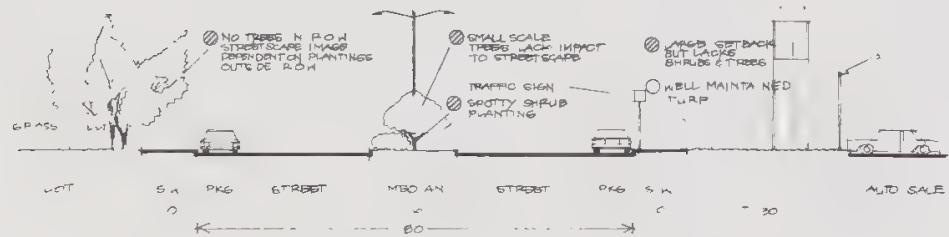


DIAMOND BOULEVARD

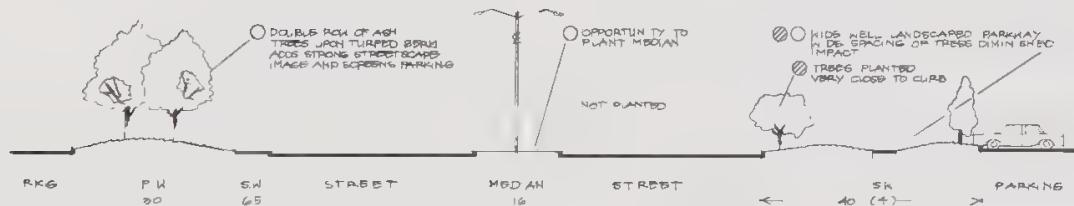
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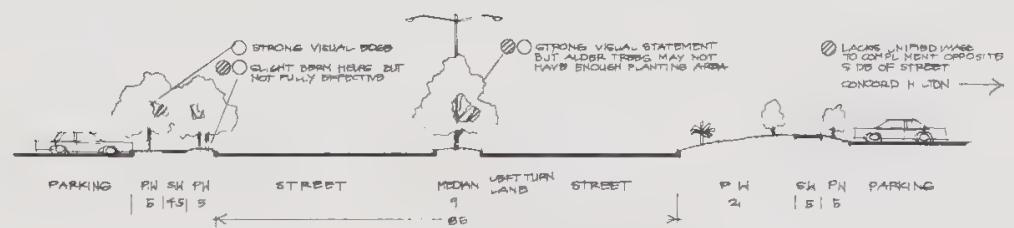
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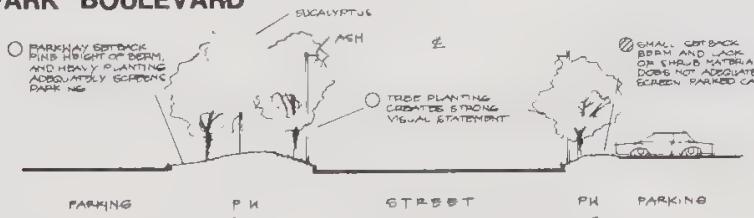


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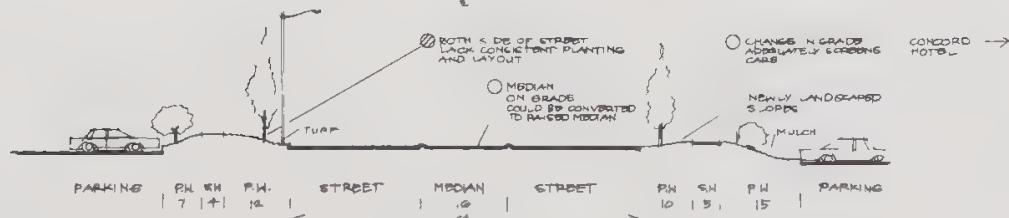


MERIDIAN PARK BOULEVARD

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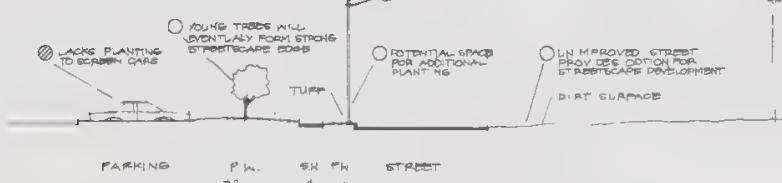


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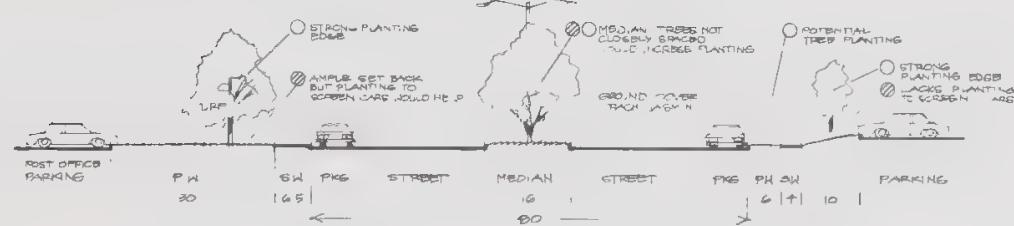


DIAMOND/MERIDIAN PARK

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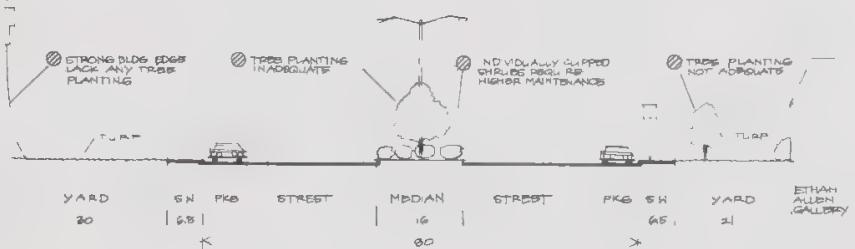


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GALAXY WAY

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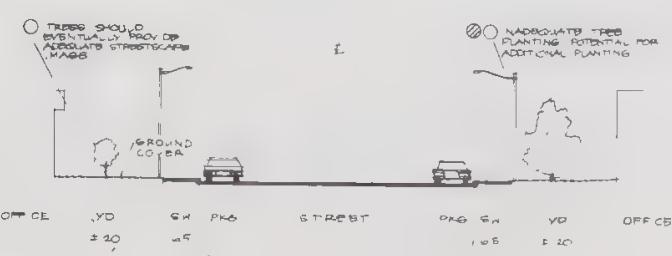
COMMERCE LANE

31



GALAXY WAY

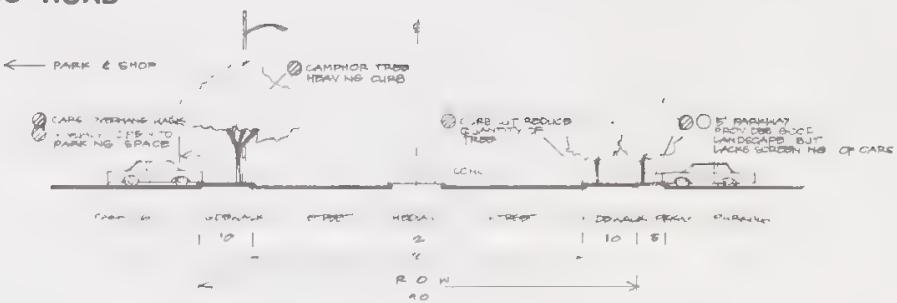
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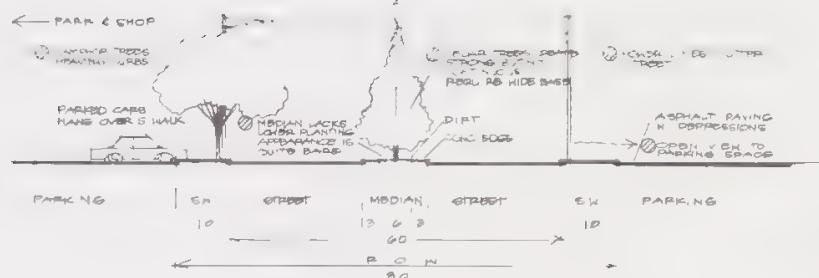
WILLOW PASS ROAD

DOWNTOWN

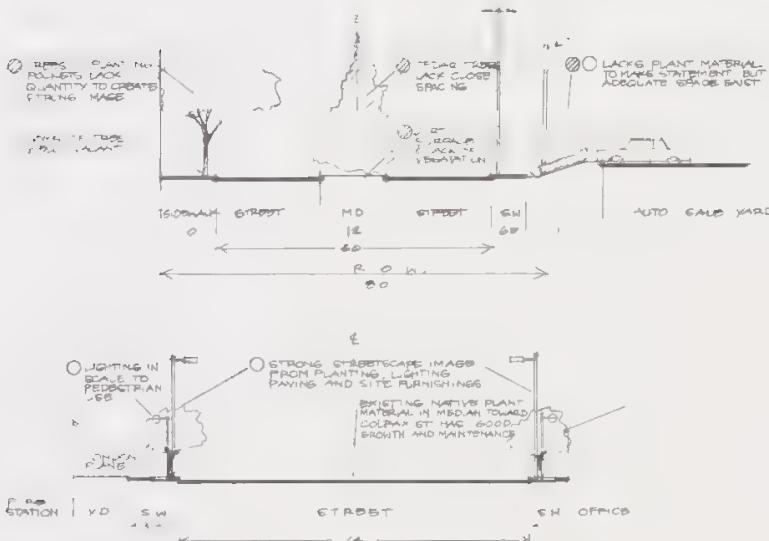
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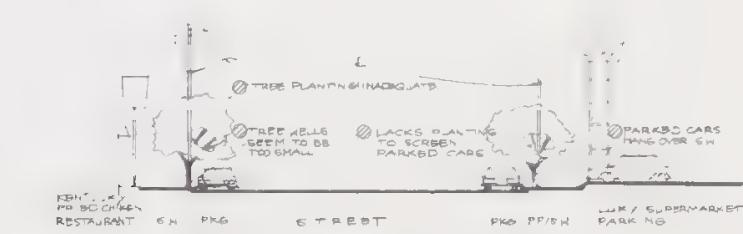
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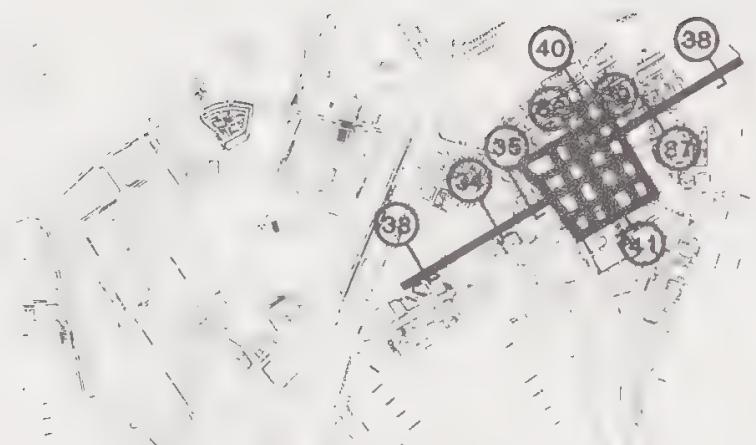
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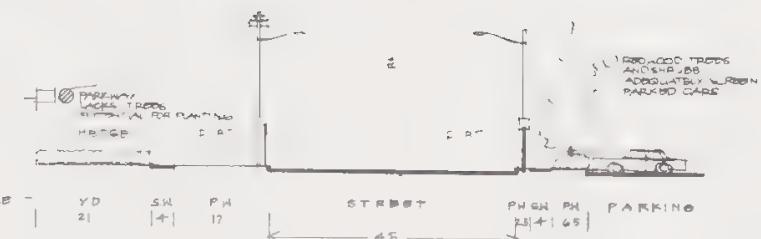


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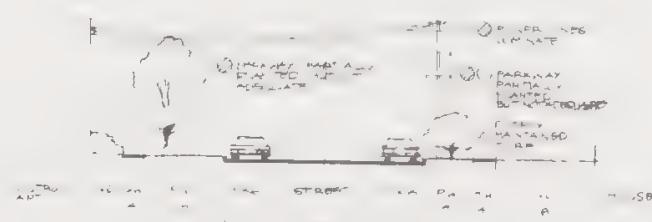
COLFAX STREET

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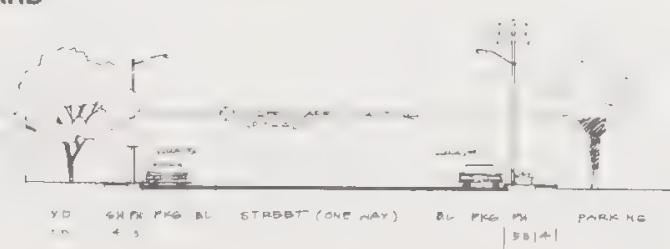
BONIFACIO

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CONCORD BOULEVARD

41



MAINTENANCE DISTRICT BROCHURE PUBLISHED BY CITY OF CONCORD

INFORMATION ABOUT THE PROPOSED DOWNTOWN LANDSCAPE MAINTENANCE DISTRICT

November 30, 1983

The City of Concord is proposing to establish a Downtown Landscape Maintenance District to pay for the maintenance of landscaping in the downtown area. This brochure is designed to answer any questions you might have about the proposal.

Purpose

The purpose of the District will be to provide the highest level of landscape maintenance to make the area more attractive for business and to add to the quality of life in Concord.

All work done by the District will be supervised by persons technically qualified to implement the procedures, including a part-time expert with a degree in ornamental horticulture.

District Boundaries

The District will include the "high visibility" areas of Downtown Concord bounded by Freeway 680 on the west and Port Chicago Highway/Bay Area Rapid Transit tracks on the east. The northern boundary is approximately 100 feet north and parallel to Concord Avenue/Pacheco Street and the southern boundary approximately 200 feet south and parallel of Willow Pass Road/Clayton Road, including Galindo as far south as Cowell Road. The proposed boundary map may be viewed at the Public Works Department, Civic Center, 1950 Parkside Drive, Monday through Friday, 8:00 a.m. to 5:00 p.m.

Areas of Responsibility

The City of Concord Redevelopment Agency Board has pledged approximately \$125,400 to replace existing non-functional irrigation systems and improve landscape installations within the District to accommodate the high level of landscaping envisioned.

The District will be responsible for all landscaped areas in the public right of way within the boundary of the District. This will include but not be limited to mowing, trimming, weed, disease and pest control, aerating and renovating, watering and fertilizing all lawn areas. For the trees, shrubs and

ground cover areas, the maintenance will include watering, pruning, pest control, weeding, staking, guying and fertilizing, among other things.

Rock areas in the median shall be kept clean and free of weeds. Trash removal in the entire area will be on a daily basis.

Landscape and Lighting Act

It is proposed that the Maintenance District be established under the Landscape and Lighting Act of 1972. This 1972 Act provides for the levy and collection of assessments to cover the cost of maintaining public landscape or street lighting improvements. Total costs are then assessed to the property within the district in relation to the benefit gained. In this regard, it is evident that those properties fronting on landscaped streets would gain the most benefit and, thus, would incur the higher assessment. The district was divided into three zones to spread the assessment more easily according to benefit derived. For example, the Concord Avenue/Willow Pass Road/Clayton Road landscaping outside of Zone 1 (area around the Plaza) benefits all property within the district but especially the properties fronting on those streets. It is proposed that two-thirds of those maintenance costs be spread by area to the entire district and an additional one-third to properties fronting on those streets.

Assessment

Property owners may determine their estimated assessment for the 1984-85 year by knowing the zone in which the property lies, the size (square feet) of the property and the front footage of the property. With this information and the rate for that particular one, it is an easy matter of multiplication and addition to determine the assessment.

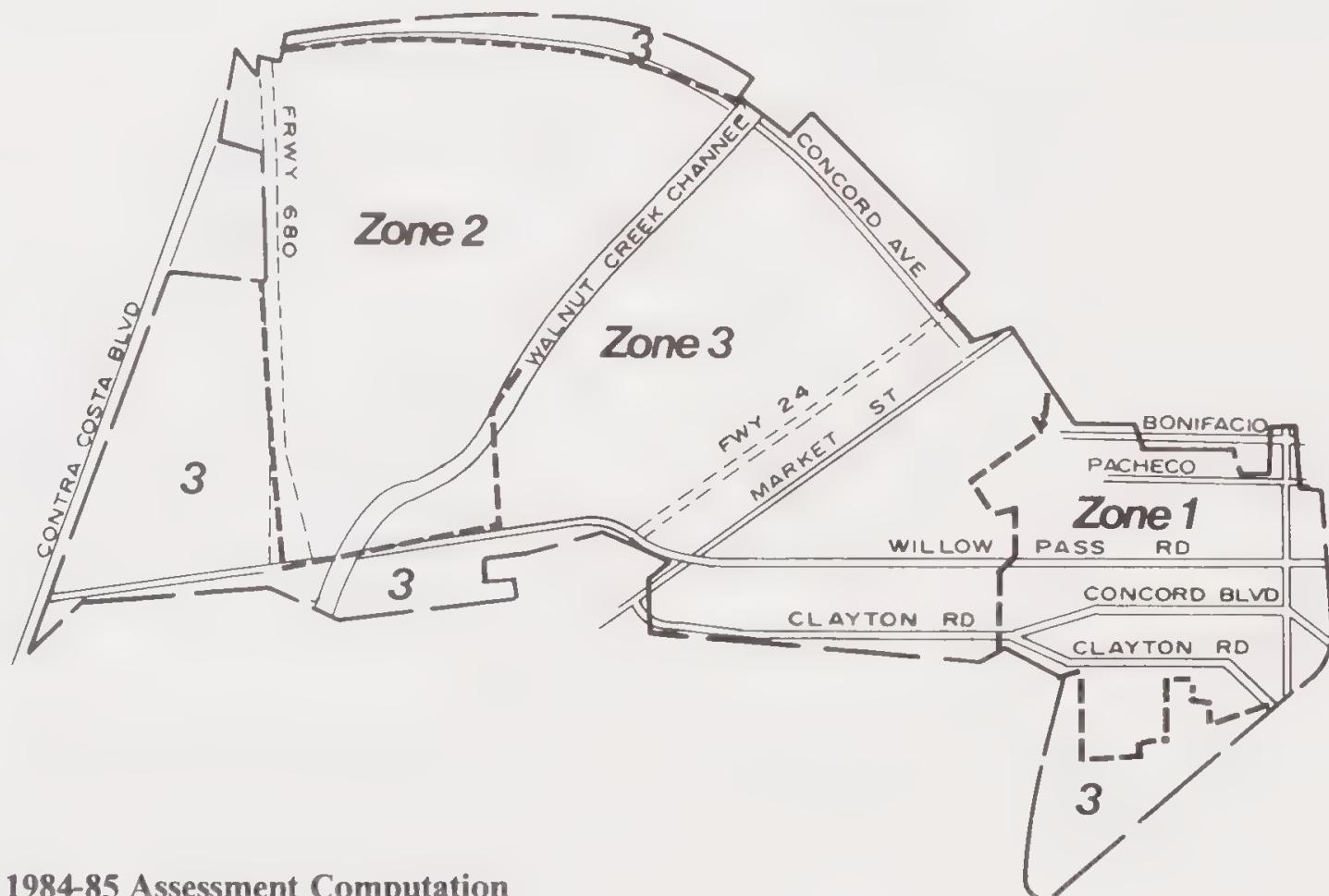
If the proposed district is approved, the City Council will be required to adopt and approve an annual budget every year. This total budget then will be assessed on the properties within the district and collections will be made with the annual tax bill.

City of Concord

Questions?

Any questions regarding procedures or assessments may be handled by calling the City offices at 671-3131.

Downtown Landscape Maintenance District Boundary



1984-85 Assessment Computation

All parcels within the District (Area sq. ft. x .00074)

PLUS

Zone 1 (Area sq. ft. x .026) + (Front footage x .303)

OR

Zone 2 (Area sq. ft. x .0045) + (Front footage* x 2.14)

Plus

(Front footage Willow Pass/Clayton x .274)

or

(Front footage Concord Avenue x .59)

OR

Zone 3 (Front footage Willow Pass/Clayton Road x .274)

or

(Front footage Concord Avenue x .59)

*Other than Willow Pass/Clayton Road/Concord Avenue.

Tentative Schedule of Events

Engineer files estimate
of assessments for each property
within the District.

February 1984

Council accepts engineer's
report and adopts Resolution
of Intention to form District.

April 1984

Public Hearings on all or any
protests. Decision is made
by City Council whether to
form or abandon the District.

May or June 1984

MAINTENANCE SPECIFICATIONS

A. Workmanship, Quality and Appearance Level

1. It is the intention of the City to require the highest level of quality in landscape maintenance compatible with standard practice.
2. All work under this agreement shall be supervised by personnel who are technically qualified and possess those management skills required to implement modern methods and newly-developed horticulture procedures. A landscape maintenance expert with a degree in Ornamental Horticulture shall be on the site for a minimum of four (4) hours per week. Contractor's employees, while on the premises, shall present a neat, clean appearance. Contractor shall provide, and employees are to wear, a standard shirt which includes Contractor's insignia. In addition, Contractor's vehicles shall bear Contractor's name and insignia.
3. The Contractor shall perform the work described herein under the direction of and to the satisfaction of the Inspector. The Inspector may make inspections at any time and request from the Contractor additional work or services if required. The Contractor shall cooperate with the City Inspector to enable him to determine the Contractor's conformity with these Specifications. The Contractor shall be available for consultation with the Inspector.
4. The Contractor shall supply to the City two emergency telephone numbers for on-call emergencies 24 hours per day, 7 days per week.

B. Materials

Fertilizer: Shall be a complete fertilizer furnishing the required percentage of nitrogen, phosphoric acid and potash to keep lawns,

trees, shrubs and other plants in a healthy and vigorous growing condition. Applications shall be made at recommended rates.

If the application of iron, zinc or other trace minerals is required to correct a specific soil deficiency, it shall also be applied as specified by the City Inspector at the Contractor's expense.

Insecticides, Fungicides, Herbicides and Rodenticides: Shall be of the best quality obtainable, brought to the job site in the original manufacturer's containers, properly labeled with guarantee analysis. 2-4-D will not be allowed in the Downtown Plaza area or where people or pets may come in contact.

Spraying for insects, disease and weed control shall be done only by qualified, trained personnel, under the supervision of a State licensed pest control operator, using recognized and approved materials. The spraying shall be done with extreme care to avoid any hazard to any person or pet in the area or adjacent areas, or any property damage. The Contractor shall send a duplicate copy of the State Pesticide Report to the Inspector each month.

Tree Stakes, Tree Ties and Guy Wires: Shall be of materials to match those existing on the site, or as directed by the Inspector. Staking shall be single stake with two "rubber hose" type ties with No. 12 wire.

Replacement of Trees, Shrubs, Ground Cover and Other Plants: Replacement of trees and plants shall be of a size, condition and variety acceptable to the Inspector.

Lawn Seed for Reseeding: Shall be a mixture of 80% Manhattan Ryegrass and 20% Kentucky Bluegrass "Fylking". Lawn seed shall be certified.

C. Lawn Care

The Contractor shall maintain all lawns in a healthy growing condition by performing the following operations and other work incidental thereto:

Mowing: Lawns shall be mowed once a week, or as necessary, to maintain a neat, trim appearance. The cutting edges of all mowing equipment shall be kept in a sharp condition. If reel-type mowers are used, they shall be kept in proper adjustment. Bruising or rough cutting of grass will not be permitted. Mowers will also be adjusted and operated so that the grass is cut at a uniform height. "Scalping" of high places will not be permitted. Papers, rubbish and debris shall be removed by the Contractor prior to mowing. Lawns shall be cut to a height of 1½ inches from November through February and to a height of 2 inches from March through October, unless otherwise noted.

Trimming: All edges shall be trimmed once a week after each cutting. This trimming shall include cutting all grass along walls, fences, foundations, curbs, sidewalks, shrubs, tree trunks, poles, guywires or any other object within or immediately adjacent to the lawn areas.

1. The trimming shall be done by power edgers or by hand, but in no case will soil sterilants or other types of weed killers be permitted without prior approval of the Inspector.
2. Trim around sprinkler heads, as necessary, to a maximum of 1 inch beyond edge of sprinkler to provide maximum water coverage.
3. Care shall be taken to avoid damage to tree trunks, shrubs, sprinklers, buildings and other structures. Damage shall be reported to the Inspector and repairs promptly made.

Weed, Disease and Pest Control: Lawns shall be kept free of weeds. Weeding may be done manually or by the use of selective weed killers. Extreme caution shall be observed, if selective weed killers are used, not to damage any other plants. If spraying is done, it shall only be done at times when there is no wind.

1. Insects and diseases shall be controlled by the use of approved insecticides and fungicides. 2-4-D shall not be allowed in areas where the public may come in contact with it.
2. Moles, squirrels and gophers shall be controlled by Inspector approved methods. Poison baits must be placed so as not to create a hazard to children or pets.
3. No insecticides, rodenticides, fugicides or herbicides shall be applied without prior consent and approval of the Inspector.
4. The Contractor shall send a duplicate copy of the State Pesticide Report to the City Inspector each month.

Aerating and Renovating: Lawns shall be aerated at least twice a year, Spring and Fall, with a plug-type machine. Lawns shall also be fertilized when aerated. This shall be done more frequently if required to maintain good water penetration. Renovating, where required by the Inspector, shall consist of verticutting and thatch removal, followed by reseeding.

1. Depressions shall be brought level to grade with top soil and reseeded as necessary.
2. Bare places shall be reseeded as necessary.

Watering: Lawns shall be deeply watered as weather conditions require to provide adequate moisture for optimum growth. Lawns shall at no time show a lack of fresh green color or a loss of resilience due to lack of water.

1. Wherever the permanently-installed sprinkler system does not adequately cover the areas, the Contractor shall supply and set out additional hoses and sprinklers as required to uniformly water all areas.
2. Watering shall be done at night or in the early morning.
3. Watering shall be controlled to prevent excessive run-off, ponding and over-watering.

Fertilizing: Lawns shall be fertilized six times per year. Application shall be at a rate of 8 lbs. of actual nitrogen per 1,000 square feet per year. Apply 16-6-8 turf fertilizer (best or equivalent) four times per year and Ureaform 38-0-0 (or equivalent) twice per year.

1. Lawns shall be watered prior to, and immediately after, fertilization to prevent burning of grass.
2. Burning of grass due to improper fertilization shall be considered as "unsatisfactory work" and shall be handled as property damage as noted in these Specifications.
3. The Contractor shall give adequate notice to the Inspector prior to the time of fertilizing. The Inspector will inspect the fertilizer material and the method and rate of application to ensure compliance with these Specifications.

D. Trees, Shrubs, Ground Cover and Plant Care

Maintain trees, shrubs and ground cover in a healthy growing condition by performing the following operations and other work incidental thereto:

Watering: Water deeply and slowly to establish moisture to the full depth of the root zone. Watering shall be done in a manner to avoid erosion, excessive run-off, ponding or creation of a water-logged soil condition. Contractor's hoses and and sprinklers shall be used to supplement the sprinkler system where necessary, to ensure complete coverage.

1. Plants should not be watered until a moisture check has been made of representative plants in the landscape. Use a probe or other tool to check the moisture in the root ball, as well as the soil surrounding the root ball. Also, a need for water should dictate the frequency of watering by an automatic or manual sprinkler system.
2. Maintain a large enough water basin around plants so that enough water can be applied to establish moisture through the major root zone. When hand watering, use a water wand to break the water force. In rainy season, open basins to allow surface drainage away from the root crown where excess water may accumulate.

Pruning: All pruning shall be done by qualified professional personnel using recognized and approved methods and techniques.

1. Excessive pruning or stubbing back will not be permitted.
2. All cuts over 2 inches in diameter shall be painted with approved tree wound dressing, such as asphaltic sealers.

3. All pruning cuts shall be made flush. They shall be cleanly cut, with no tearing of the bark.
4. Shearing of shrubs and ground cover will not be permitted, unless specifically approved by the Inspector.
5. Trees shall be pruned to select and develop permanent scaffold branches that are smaller in diameter than the trunk or branch to which they are attached, which have vertical spacing from 18 to 48 inches and radial orientation so as not to overlay one another.
6. Under no circumstances will stripping of lower branches ("raising up") of young trees be permitted. Lower branches shall be retained in a "tipped back" or pinched condition with as much foliage as possible to promote caliper trunk growth (tapered trunk). Lower branches can be cut flush with the trunk only after the tree is able to stand erect without staking or other support.
7. Trees shall be thinned out and shaped, when necessary, to prevent wind and storm damage.
8. Pruning of trees and shrubs shall be done as needed to achieve the following:
 - a. To shape, particularly to correct misshaping caused by the wind.
 - b. To raise the lower branches of trees above head height wherever they overhang walks.
 - c. Street trees shall be maintained in accordance with the City codes for height clearance.
- d. Pruning of trees to maintain height clearances shall be done with the intent of encouraging an ascending branch structure to achieve required clearance in travel area without entirely removing all lower branches.
- e. To cut back shrubs and ground cover where they encroach on the walks and the paved areas.
- f. To cut back branches that are rubbing on walks, fences and buildings.
- g. To remove suckers, watersprouts and other undesirable growth on trees.
- h. To remove all dead or damaged branches.
9. Major pruning of deciduous trees and shrubs shall be done during the dormant season. Minor pruning may be done at any time. Pruning to remove a hazard shall be done immediately.
10. Ground cover shall not be permitted to climb on walls, trees, shrubs and other structures.

Pest Control: Spraying for insects, disease and weed control shall be done only by qualified, trained personnel, under the supervision of a State licensed pest control operator, using recognized and approved materials. The spraying shall be done with extreme care to avoid any hazard to any person or pet in the area or adjacent areas or any property damage.

1. Snails and slugs shall be controlled by the use of any approved non-arsenical, methaldehyde bait. A liquid snail and slug bait shall be used in any area where children or pets may be expected.

2. In no case will extremely toxic materials such as arsenicals, parathion, TEPP, dieldrin, etc. be permitted.
3. No chemicals may be applied without the prior consent and approval of the Inspector.
4. The Contractor shall send a duplicate copy of the State Pesticide Report to the Inspector each month.

Weeding: Tree, shrub and ground cover areas shall be kept free of weeds.

1. Weeding may be done manually or by the use of selective weed killers. Extreme caution should be observed, if selective weed killers are used, not to damage any other plants. Spraying shall be done only at times when there is no wind.
2. Grassy weeds shall be controlled by spraying with an herbicide as needed.
3. Copies of all herbicide spray reports must be given to the Inspector at the beginning of each month.

Staking and Guying: Maintain and replace stakes and guys with equal material. Maintain and replace plant ties to provide support without chafing of the bark. Additional stakes and/or ties shall be placed from time to time to correct misshaping caused by wind.

Fertilization: Fertilize all trees, shrubs and ground cover twice a year with 16-6-8 or another approved fertilizer. Fertilizer shall be applied in the Spring and the Fall at the following rates at each application: 1/4 lb. per shrub and 1/2 lb. per tree of less than 1 inch trunk diameter; 3/4 lb. per inch per tree for trees with more than 1 inch trunk diameter; 20 lbs. per 1,000 square feet of ground cover.

1. Avoid applying fertilizer to the root ball and base of main stem; rather, spread evenly under plant to drip line.
2. The Contractor shall give adequate notice to the Inspector prior to the time of fertilizing. The Inspector will inspect the fertilizer material and the method and rate of application to ensure compliance with these Specifications.

Replacement Material: Remove dead and damaged plants and replant with material of equivalent size, condition and variety, subject to approval of the Inspector. This includes any plant material or trees damaged.

Bare Areas: Those between shrubs and trees, or without plants, or with some type of ground cover except grass; i.e., rocks, bark, ivy, etc., shall be kept free of weeds, either manually or by the use of selective weed killers.

E. Irrigation System

Before beginning the maintenance program, the Contractor and the Inspector shall together inspect all of the existing irrigation systems. The Contractor shall be responsible for the efficient operation of the irrigation system during the contract period. Specifically, he shall:

1. Adjust all heads and trim around heads to maintain proper coverage and eliminate overspray.
2. Adjust control valves to eliminate misting.
3. Remove last head from each system and flush lines if required.
4. Repair and replace any equipment damaged as a result of contract operations, at the Contractor's expense. Such repairs shall be made within one watering period.

5. Damage not resulting from the Contractor's negligence excepting riser up to and including sprinkler head shall be reported promptly to the City Inspector, together with an estimate of costs for correction of the condition.
6. Where the installed sprinkler system does not cover the area adequately, the Contractor shall provide his own sprinklers and hoses to adequately water the area.
7. In late Winter, Contractor shall check all systems for proper operation. Lateral lines shall be flushed out after removing the last sprinkler head or two at each end of the lateral. All heads are to be adjusted, as necessary, for unimpeded coverage.
8. The Contractor shall set and program automatic controllers for seasonal water requirements. The Contractor is responsible to keep the automatic controller systems in working order. The Inspector shall have the authority to dismiss unqualified controller servicemen. Water on the roadway from irrigation, is not permitted.
9. Controllers shall be programmed to apply water in short multiple intervals to prevent runoff.
10. Controllers shall be programmed to apply the minimum amount of water required to maintain plant material in a healthy vigorous condition.

F. Annuals

Annuals shall be planted, maintained and replaced by performing the following operations and other work incidental thereto:

1. Locations: Annuals shall be planted and maintained in existing areas and as directed by the City Inspector.
2. Type: The type and variety of annuals shall be as specified by the City Inspector.
3. Replacement: Annuals shall be changed/rotated three times a year. Replacement plants shall be in 4 inch pots, in full color,

and in a healthy thriving condition. Plants shall be spaced at 6 to 8 inches apart. Annuals shall be inspected at least twice a week and all dead or dying plants shall be replaced.

4. Fertilizer: Fertilizer for the annuals shall be a low nitrogen, high potassium, high phosphorus type, such as bone meal or Grow Power and shall be approved for use by the Inspector. Fertilizer shall be added to the soil and mixed thoroughly before the plants are planted.

G. General Maintenance and Clean-up

1. Litter shall be picked up daily.
2. All clippings, trimmings, cuttings, rubbish and debris shall be removed daily from the area and disposed of at Contractor's expense. Do not use City refuse cans.
3. All ground cover areas, areas around shrubs and trees, lawns next to buildings, fences, benches, medians, islands, sidewalks, curbs and gutters shall be kept free from weeds, grasses, rocks, glass and debris. Use of spraying for weed control along sidewalks, curbs and gutters will be allowed.
4. All cracks in sidewalks, curbs, street gutters and other paved areas, including medians or other islands, shall be kept weeded. Use of a pre-emergent weed spray will be allowed.
5. Sidewalks and paved areas shall be swept and cleaned of any dirt or soil that might be washed from adjacent slopes or planted areas as required.
6. Any eroded places shall be repaired by replacement of top soil to bring them back to original grade, as required. Cause of erosion shall be reported immediately to the Inspector.
7. Any unsafe conditions shall be reported immediately to the Inspector.



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